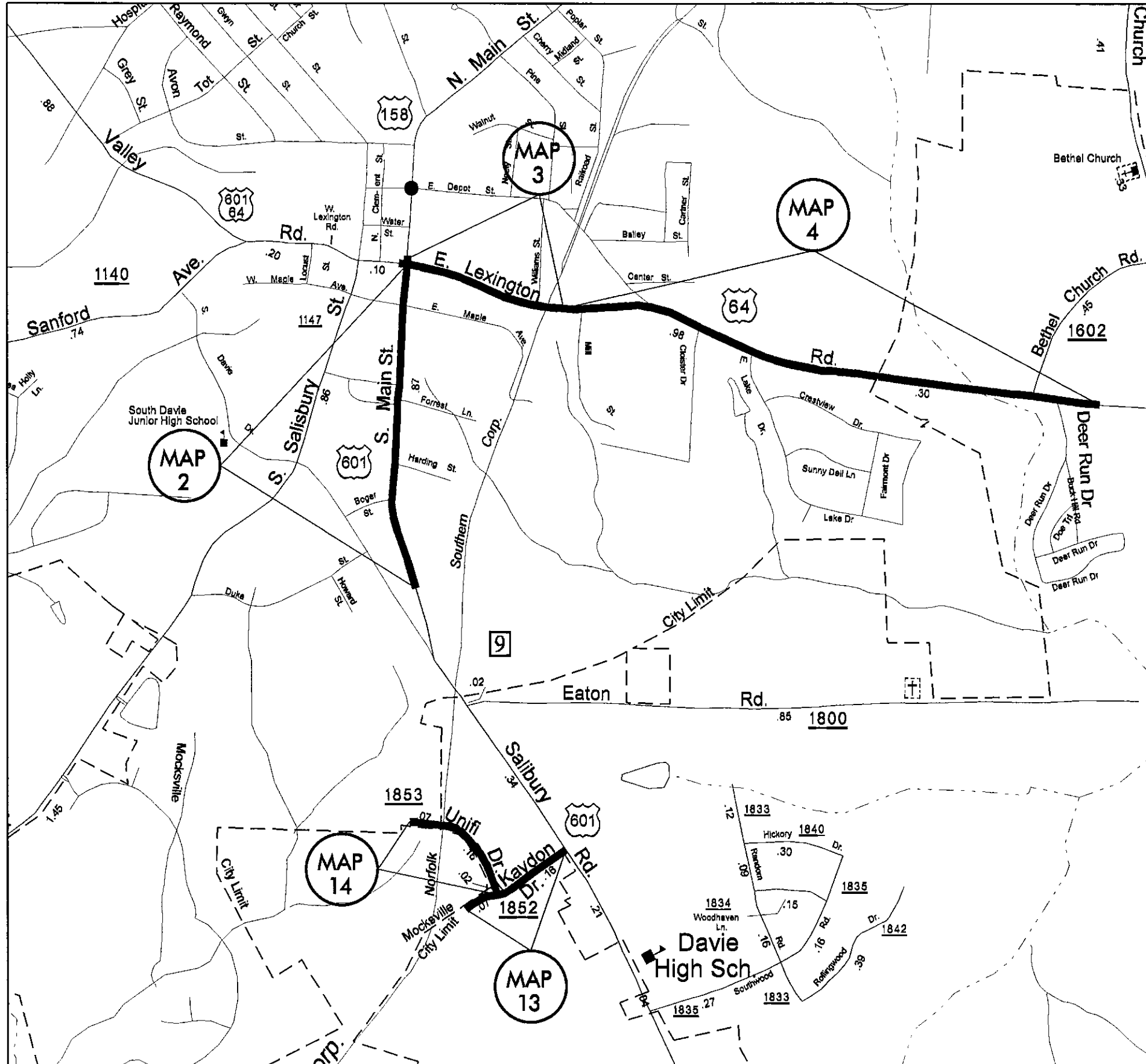


MAP 1 NC 801
Curb Mill 0-1½"

DAVIE COUNTY
NORTH CAROLINA



MAP 2
US601
 Begin at edge of pavement of US64 intersection.
 Mill 2" Depth full width of pavement at curb.
 Pave back 2" S9.5B.
 Mill 2" Depth from curb to end of Map approximately 1200 feet north of RxR bridge.
 AST, Single Seal then Pave back with 2" S9.5B.

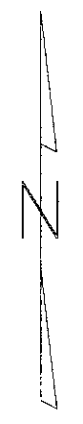
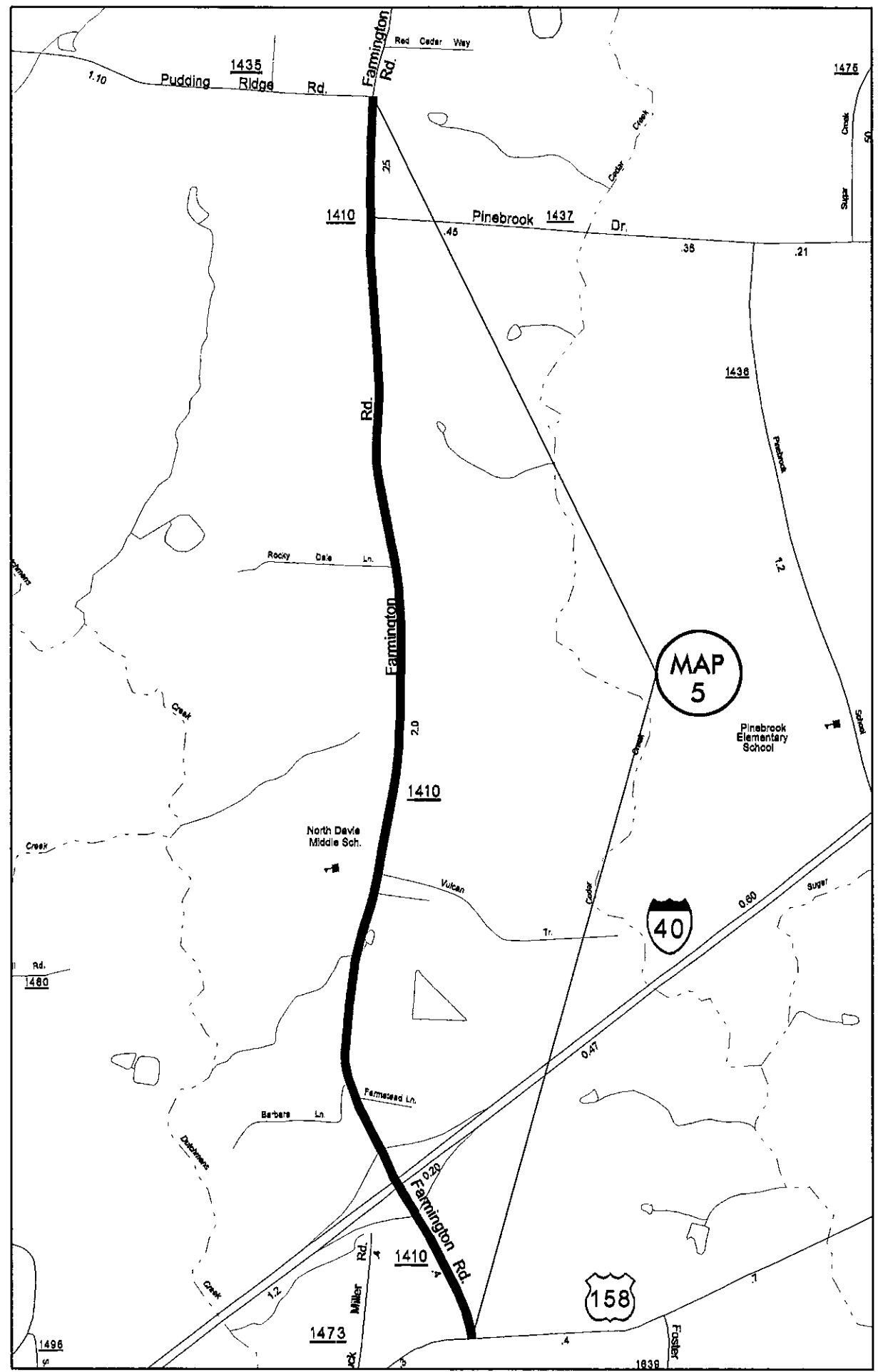
MAP 3
US64
 Mill 2" Depth from pavement jt. on west side of intersection of US601 /US158 to approximately 100 feet east of RxR bridge. 2" mill depth to include US158 loops. Pave back with 2" S9.5B.

MAP 4
US64
 Mill 0-1½" a 12 foot width at 3 lane Curb section near Charleston Ridge. Tie In Mill at end of Map at end of taper east of Bethel Church Rd. Mill and Patch a 2" depth at left turn to Bethel Church Rd. as directed by Engineer. Pave back entire Map with 1½" S9.5B

MAP 13
Kaydon Dr./CPP Global Dr. SR 1852
 Tie In Mill at US 601.
 Tie into existing surface at end of Map at End Of Maintenance Sign.

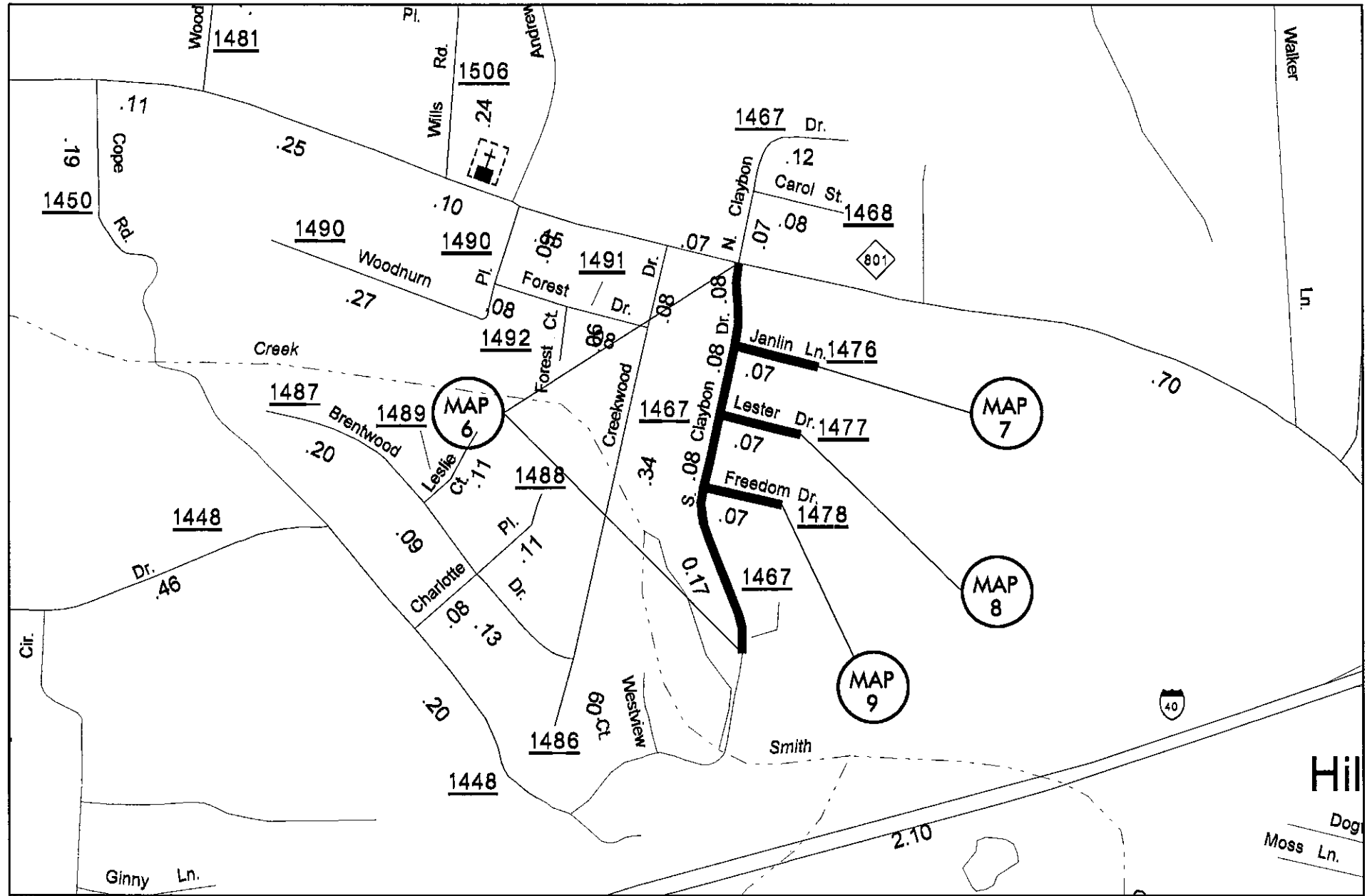
MAP 14
Unifi Dr. SR 1853
 Incidental mill approaches of RxR Do Not Mill Tie In at end of Map. Tie into new surface at CPP Global/Kaydon Dr. SR 1852

DAVIE COUNTY
 NORTH CAROLINA

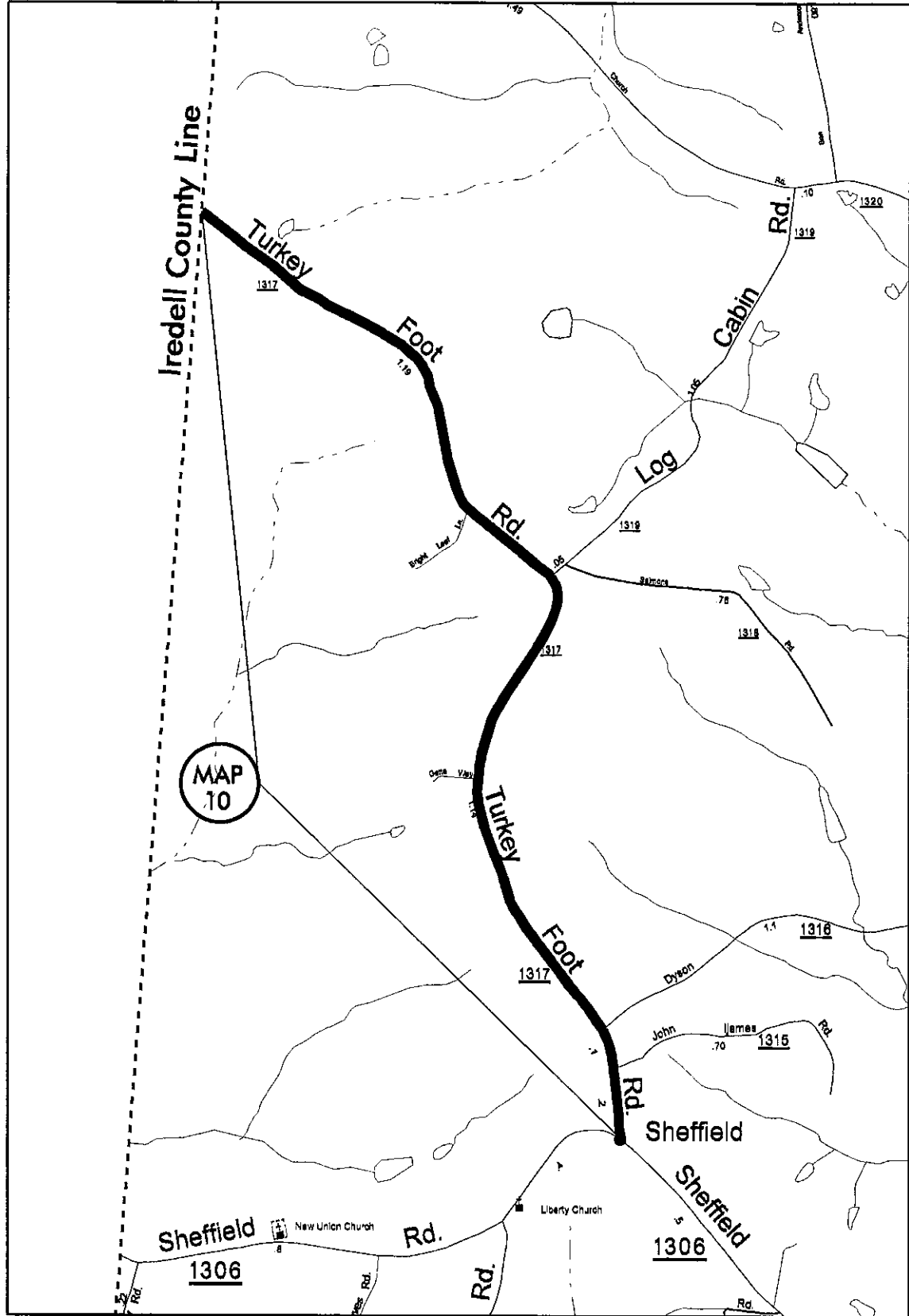


MAP NO. 5
Farmington Rd. SR 1410

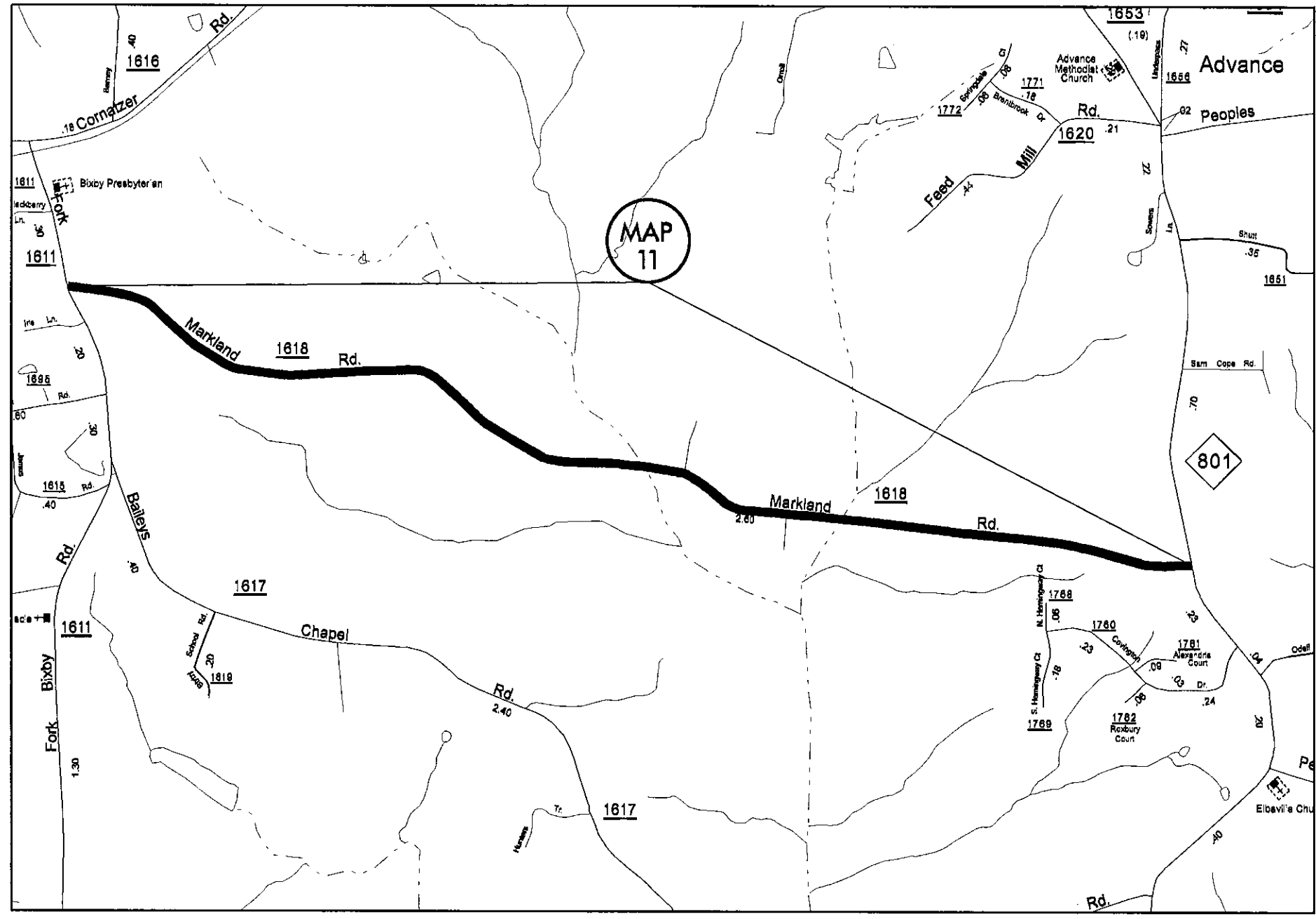
Mill 0-1½" at all curbs.
 Mill Butt Jt. at Pudding Ridge.
 Mill Asphalt 4" Depth Between US 158 to
 Approximately 390 feet north of bridge
 over I-40. **DO NOT MILL BRIDGE DECK.**
 Pave back with 2½" I19.0B.
 Pave entire Map with 1½" S9.5B.
 Overlay to include in front of New
 School.



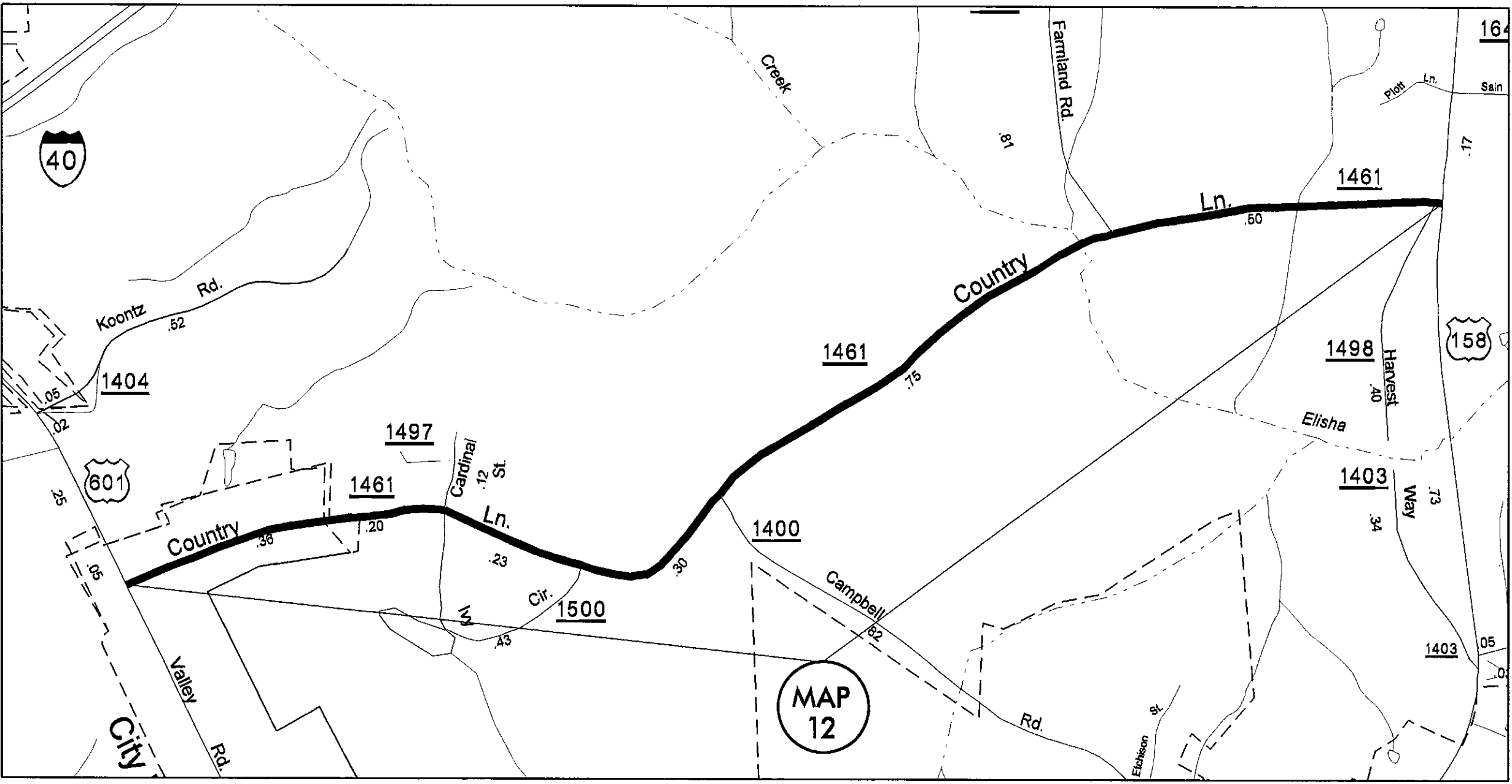
- MAP 6 Claybon Dr. SR 1467
NO Shoulder Reconstruction
NO Thermoplastic Markings
Pave 1" S4.75A
- MAP 7 Janlin Lane SR 1476
NO Shoulder Reconstruction
NO Thermoplastic Markings
Pave 1" S4.75A
- MAP 8 Lester Dr. SR 1477
NO Shoulder Reconstruction
NO Thermoplastic Markings
Pave 1" S4.75A
- MAP 9 Freedom Dr. SR 1478
NO Shoulder Reconstruction
NO Thermoplastic Markings
Pave 1" S4.75A



MAP 10
 Turkey Foot Rd. SR 1317
 Mill Butt joints at Map ends.

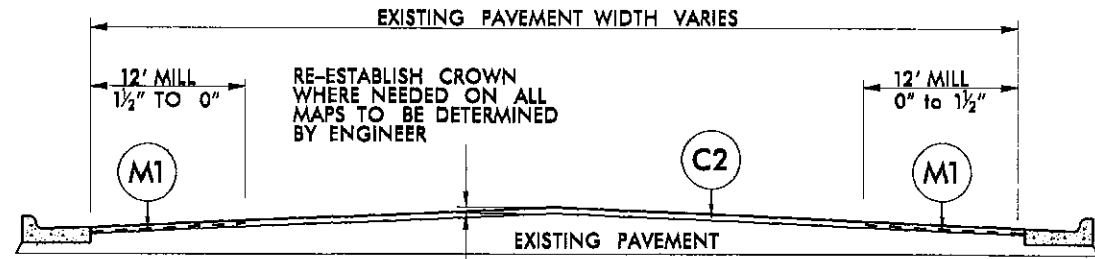


MAP NO. 11
Markland Rd. SR 1618

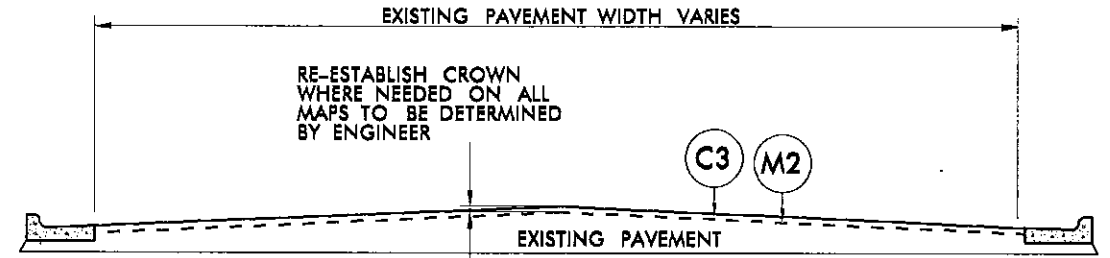


MAP 12
Country Lane SR 1461

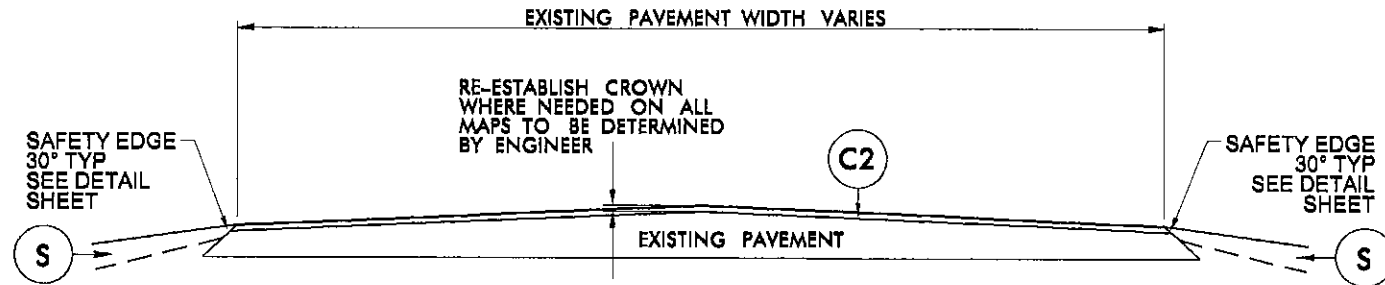
DAVIE COUNTY
NORTH CAROLINA



TYPICAL SECTION 1
MAP NO 1 NC 801
MAP NO 4 US 64
MAP NO 5 FARMINGTON RD. SR 1410



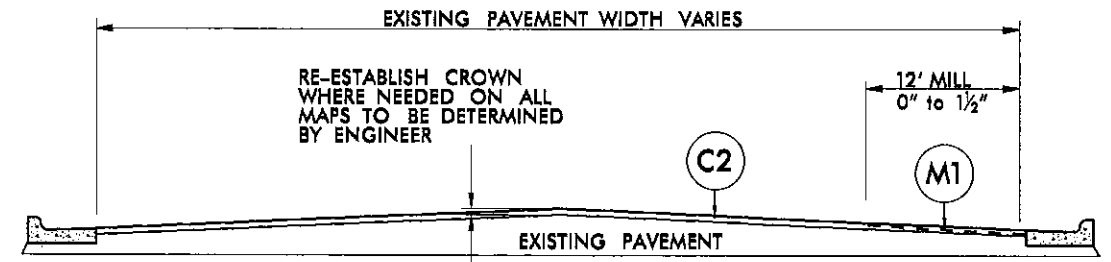
TYPICAL SECTION 5
MAP NO 3 US 64



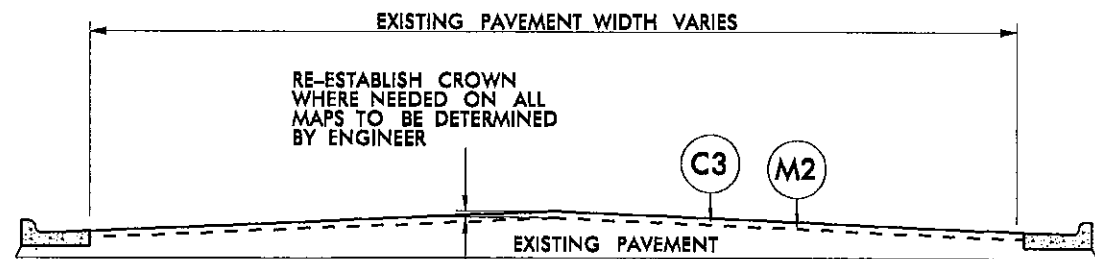
Shoulder reconstruction in areas only with no outside curb (typ.)

TYPICAL SECTION 2
MAP 1 NC 801
MAP 4 US 64
MAP 5 FARMINGTON RD. SR 1410
MAP 12 COUNTRY LANE SR 1461
MAP 13 CPP GLOBALKAYDON DR. SR 1852

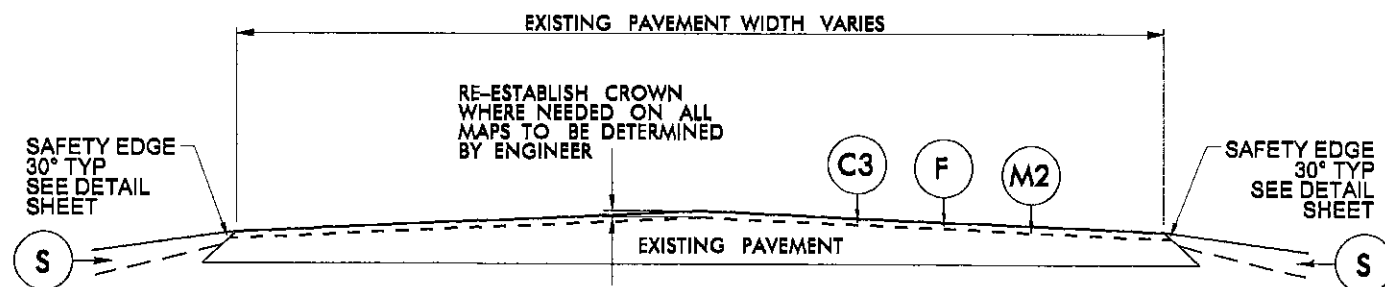
Shoulder reconstruction in areas only with no outside curb (typ.)



TYPICAL SECTION 5A
MAP 12 COUNTRY LANE SR 1461



TYPICAL SECTION 3
MAP NO 2 US 601

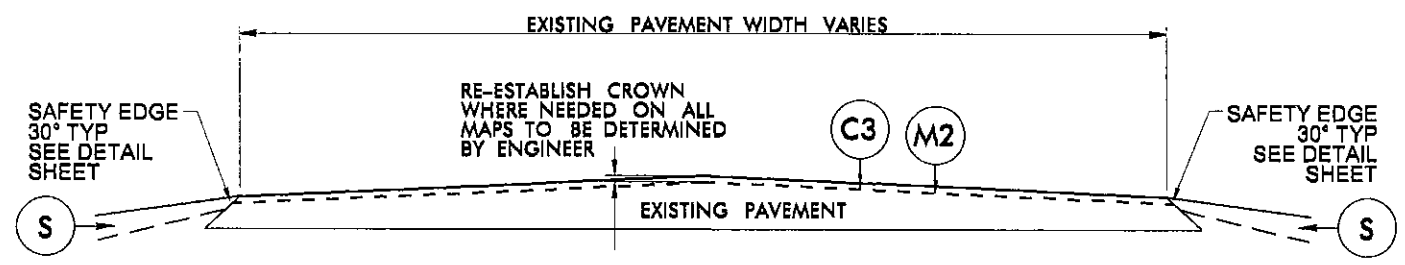


Shoulder reconstruction in areas only with no outside curb (typ.)

TYPICAL SECTION 4
MAP NO 2 US 601

Shoulder reconstruction in areas only with no outside curb (typ.)

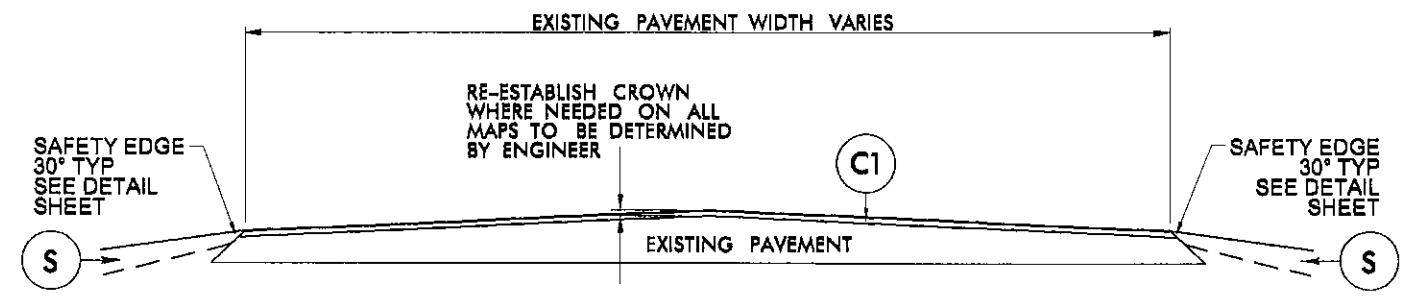
PAVEMENT SCHEDULE	
C	PROP. APPROX. 1" ASPHALT CONCRETE SURFACE COURSE, TYPE S4.75A, TO BE APPLIED AT AN AVERAGE RATE OF 100 LBS PER SQ YD.
C1	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.5A, TO BE APPLIED AT AN AVERAGE RATE OF 165 LBS PER SQ YD.
C2	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, TO BE APPLIED AT AN AVERAGE RATE OF 168 LBS PER SQ YD.
C3	PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, TO BE APPLIED AT AN AVERAGE RATE OF 224 LBS PER SQ YD.
C4	PROP. VAR. DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 112 LBS. PER SQ. YD. PER 1" DEPTH. TO BE PLACED IN LAYERS NOT TO EXCEED 2" IN DEPTH.
D	PROP. APPROX. 2 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE 119.0B, TO BE APPLIED AT AN AVERAGE RATE OF 285 LBS PER SQ YD.
F	AST, SINGLE SEAL, #78
M1	MILL ASPHALT PAVEMENT, 0 TO 1/2"
M2	MILL ASPHALT PAVEMENT, 2" DEPTH
M3	MILL ASPHALT PAVEMENT, 3" DEPTH
M4	MILL ASPHALT PAVEMENT, 4" DEPTH
S	SHOULDER RECONSTRUCTION (SEE DETAIL)
U	EXISTING PAVEMENT



Shoulder reconstruction in areas only with no outside curb (typ.)

TYPICAL SECTION 6
MAP NO 3 US 64

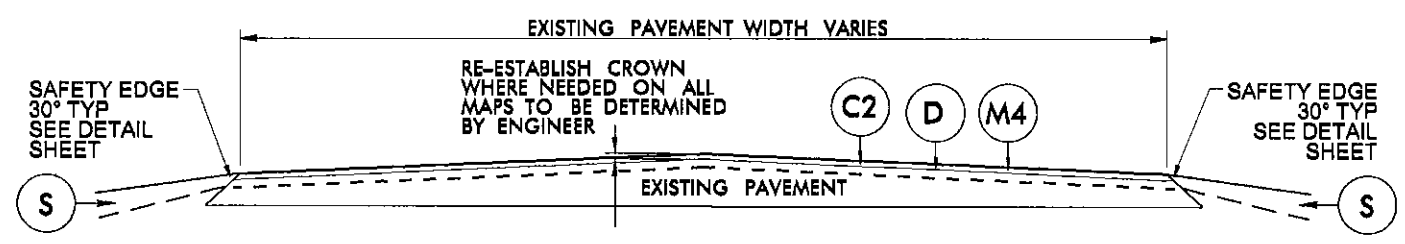
Shoulder reconstruction in areas only with no outside curb (typ.)



Shoulder reconstruction in areas only with no outside curb (typ.)

TYPICAL SECTION 8
MAP 10 TURKEY FOOT RD. SR 1317
MAP 11 MARKLAND RD. SR 1618

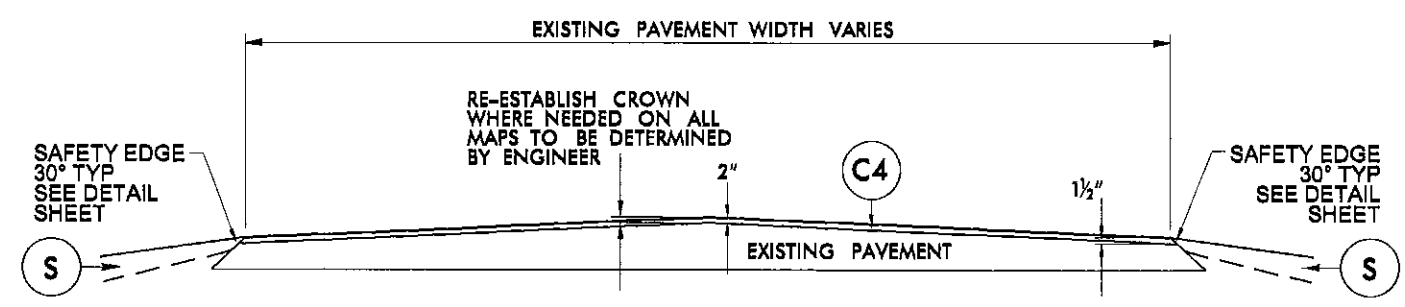
Shoulder reconstruction in areas only with no outside curb (typ.)



Shoulder reconstruction in areas only with no outside curb (typ.)

TYPICAL SECTION 6A
MAP NO 5 FARMINGTON RD. SR 1410

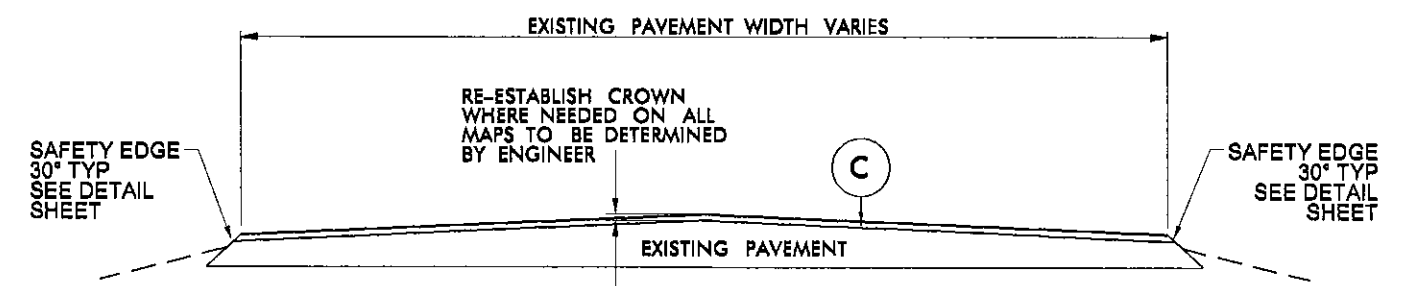
Shoulder reconstruction in areas only with no outside curb (typ.)



Shoulder reconstruction in areas only with no outside curb (typ.)

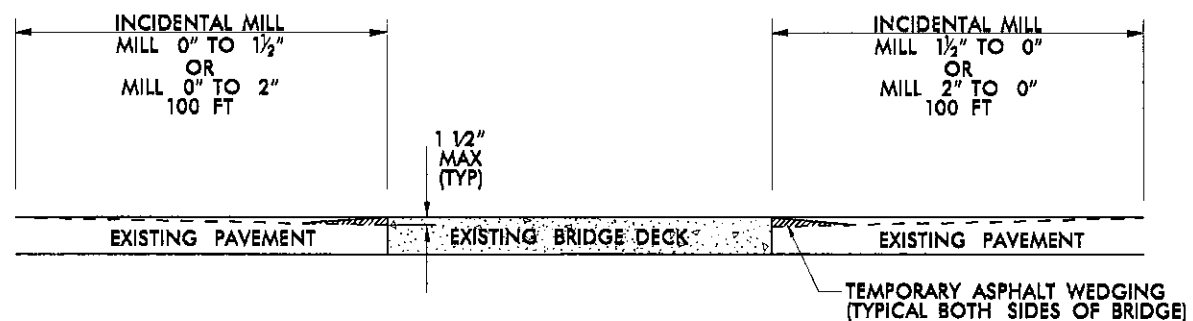
TYPICAL SECTION 9
MAP 13 CPP GLOBAL DR./KAYDON DRIVE SR 1852
MAP 14 UNIFI DR./AVGOL DR. SR 1853

Shoulder reconstruction in areas only with no outside curb (typ.)

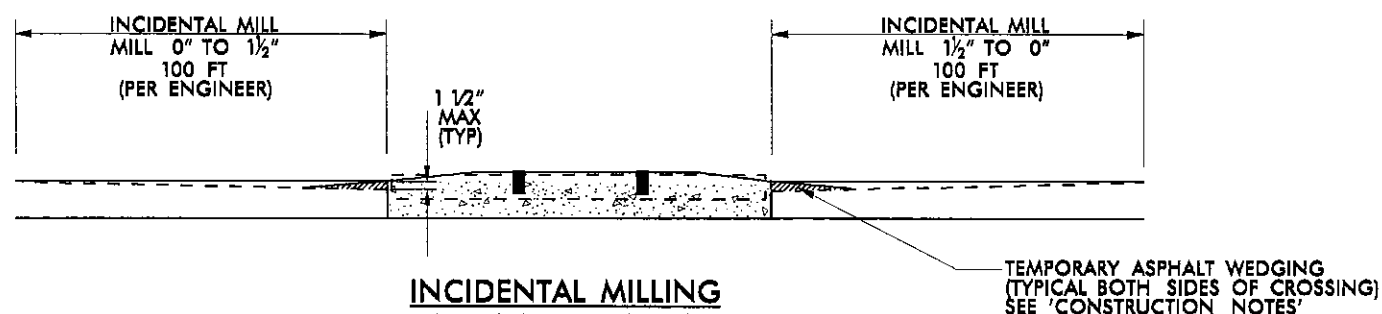


TYPICAL SECTION 7
MAP NO 6 SOUTH CLAYBON DR. SR 1467
MAP NO 7 JANLIN LANE SR 1476
MAP NO 8 LESTER DR. SR 1477
MAP NO 9 FREEDOM DR. SR 1478

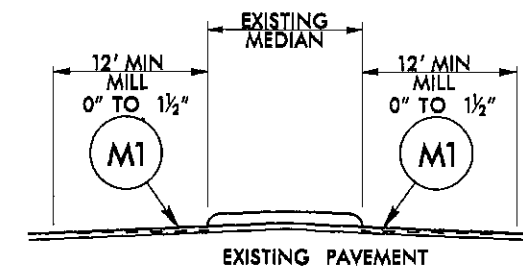
PAVEMENT SCHEDULE	
C	PROP. APPROX. 1" ASPHALT CONCRETE SURFACE COURSE, TYPE S4.75A, TO BE APPLIED AT AN AVERAGE RATE OF 100 LBS PER SQ. YD.
C1	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.5A, TO BE APPLIED AT AN AVERAGE RATE OF 165 LBS PER SQ. YD.
C2	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, TO BE APPLIED AT AN AVERAGE RATE OF 168 LBS PER SQ. YD.
C3	PROP. APPROX. 2" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, TO BE APPLIED AT AN AVERAGE RATE OF 224 LBS PER SQ. YD.
C4	PROP. VAR. DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 112 LBS. PER SQ. YD. PER 1" DEPTH. TO BE PLACED IN LAYERS NOT TO EXCEED 2" IN DEPTH.
D	PROP. APPROX. 2 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE 119.0B, TO BE APPLIED AT AN AVERAGE RATE OF 285 LBS PER SQ. YD.
F	AST, SINGLE SEAL, #78
M1	MILL ASPHALT PAVEMENT, 0 to 1/2"
M2	MILL ASPHALT PAVEMENT, 2" DEPTH
M3	MILL ASPHALT PAVEMENT, 3" DEPTH
M4	MILL ASPHALT PAVEMENT, 4" DEPTH
S	SHOULDER RECONSTRUCTION (SEE DETAIL)
U	EXISTING PAVEMENT



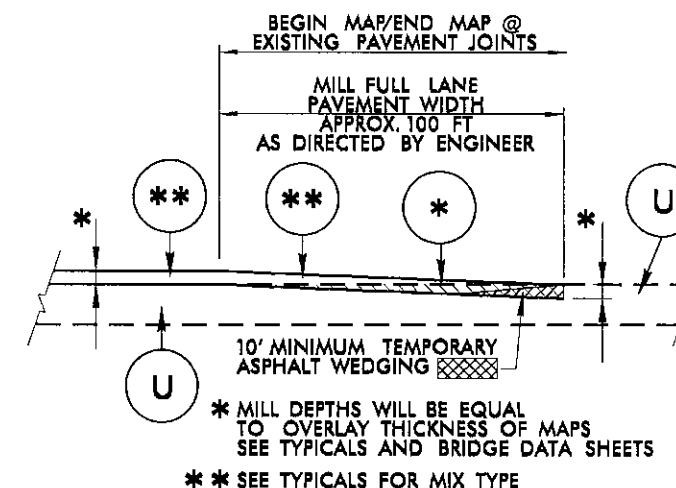
**INCIDENTAL MILLING
BRIDGE APPROACHES**
(SEE BRIDGE DATA SHEET)



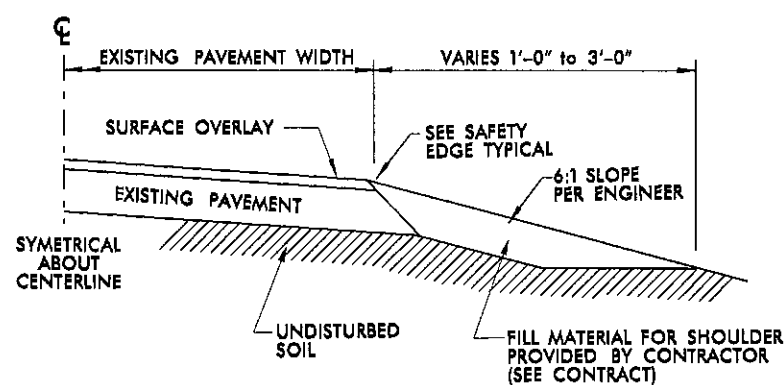
**INCIDENTAL MILLING
RAILROAD CROSSING
APPROACHES**
TEMPORARY ASPHALT WEDGING
(TYPICAL BOTH SIDES OF CROSSING)
SEE 'CONSTRUCTION NOTES'



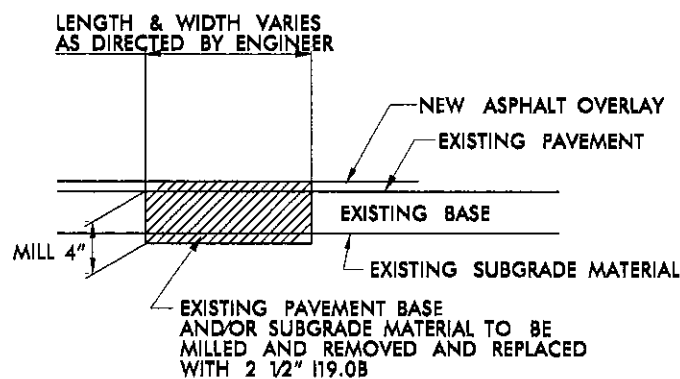
MILLING AT MEDIANS



INCIDENTAL TIE-IN MILLING DETAIL



SHOULDER RECONSTRUCTION



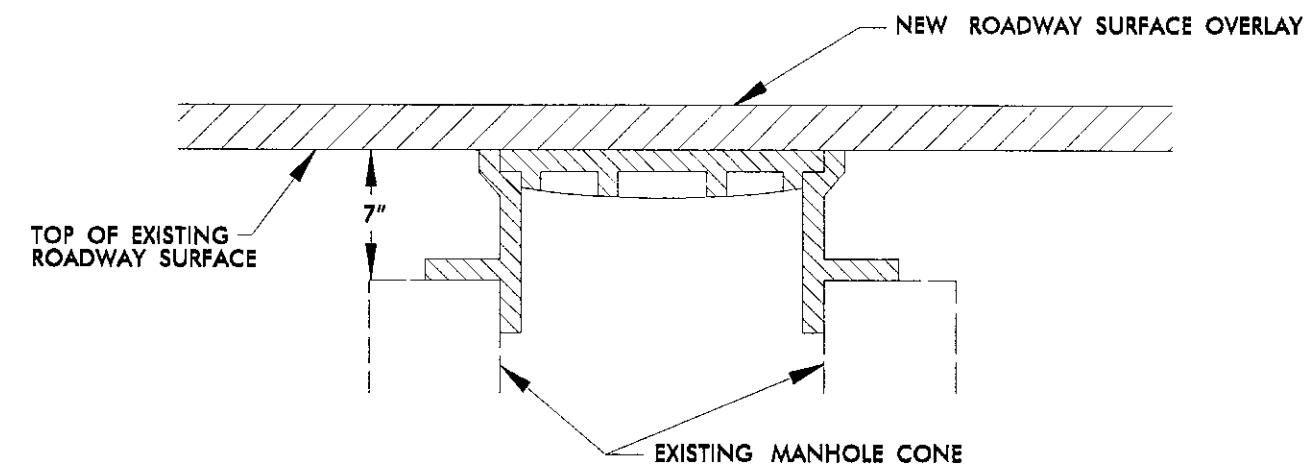
**PATCHING EXISTING PAVEMENT
PRIOR TO RESURFACING**

PAVEMENT SCHEDULE

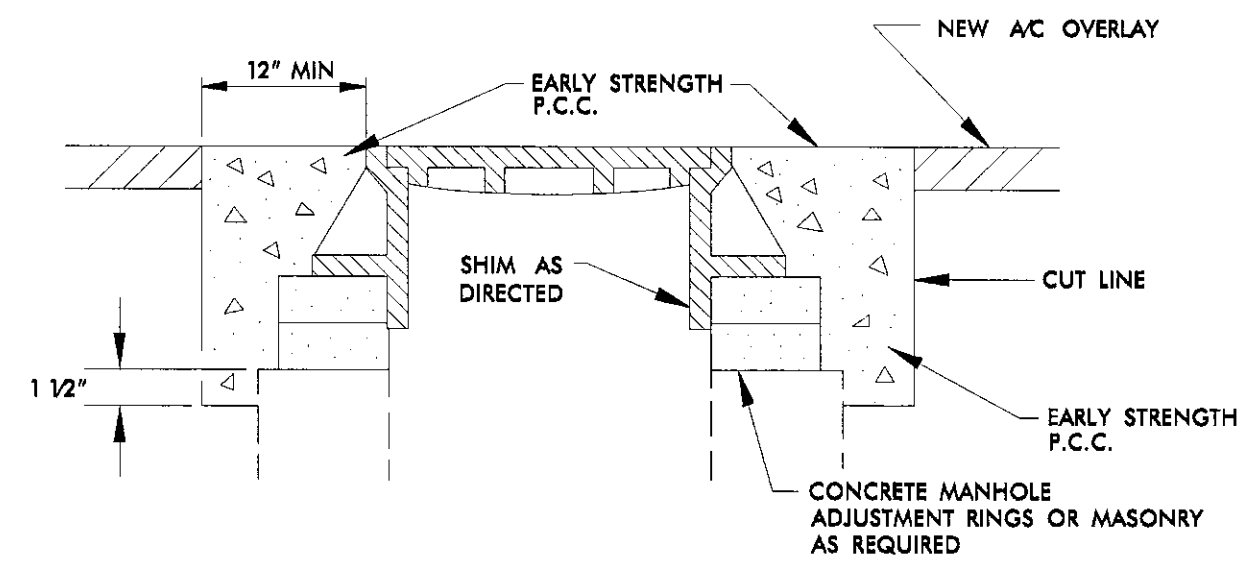
C	PROP. APPROX. 1" ASPHALT CONCRETE SURFACE COURSE, TYPE S4.75A, TO BE APPLIED AT AN AVERAGE RATE OF 100 LBS PER SQ. YD.
C1	PROP. APPROX. 1 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE SF9.5A, TO BE APPLIED AT AN AVERAGE RATE OF 165 LBS PER SQ. YD.
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C4	PROP. VAR. DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 112 LBS. PER SQ. YD. PER 1" DEPTH. TO BE PLACED IN LAYERS NOT TO EXCEED 2" IN DEPTH.
D	PROP. APPROX. 2 1/2" ASPHALT CONCRETE SURFACE COURSE, TYPE 119.0B, TO BE APPLIED AT AN AVERAGE RATE OF 285 LBS PER SQ. YD.
F	AST, SINGLE SEAL, #78
M1	MILL ASPHALT PAVEMENT, 0 to 1 1/2"
M2	MILL ASPHALT PAVEMENT, 2" DEPTH
M3	MILL ASPHALT PAVEMENT, 3" DEPTH
M4	MILL ASPHALT PAVEMENT, 4" DEPTH
S	SHOULDER RECONSTRUCTION (SEE DETAIL)
U	EXISTING PAVEMENT

CONSTRUCTION NOTES:

1. ALL QUANTITIES ARE "ESTIMATED" AS INDICATED IN THE "SUMMARY OF QUANTITIES".
2. CONSTRUCTION SHALL PROGRESS IN PHASES, IN THE ORDER INDICATED BELOW:
 - PHASE 1 - MILLING AND PATCHING (WHEN REQUIRED)
 - PHASE 2 - SURFACE OVERLAY
 - PHASE 3 - SHOULDER DROP-OFF REPAIR (AS NEEDED AND DIRECTED BY ENGINEER)
 - PHASE 4 - UTILITY ADJUSTMENTS (MANHOLE RING/COVER, VALVEMETER BOX RING/COVER, CATCH BASIN GRATE/COVER, DROP INLET GRATE/COVER, ETC.) WHEN REQUIRED.
3. BRIDGES THAT HAVE FLOOR DRAINS, SHALL HAVE ALL FLOOR DRAINS LEFT OPEN. EXTRA CARE SHALL BE EXERCISED IN MILLING (IF REQUIRED) AND IN PLACING THE WEARING SURFACE AROUND FLOOR DRAINS SO AS NOT TO HINDER EFFECTIVE DRAINAGE.
4. TEMPORARY ASPHALT WEDGING SHALL BE PLACED ON THE SAME DAY THAT BRIDGE AND/OR RAILROAD APPROACHES ARE MILLED (AND IF APPROACHES ARE MILLED PRIOR TO BRIDGE DECK).
5. FOR TWO-LANE ROADWAYS - IT SHALL BE UNDERSTOOD THAT TYPICALLY ON A ROADWAY MEASURING 20 FEET OR LESS IN WIDTH, THE CENTER OF THE WHITE EDGELINE SHALL BE LOCATED SIX INCHES FROM THE EDGE OF PAVEMENT ON EITHER SIDE OF THE ROADWAY; ON A ROADWAY MEASURING 22 FEET IN WIDTH, TRAVEL LANES SHALL MEASURE 10 FEET AND THE WHITE EDGELINE SHALL BE LOCATED ONE FOOT FROM THE EDGE OF PAVEMENT ON EITHER SIDE; ON A ROADWAY MEASURING 24 FEET IN WIDTH, TRAVEL LANES SHALL MEASURE 11 FEET AND THE WHITE EDGELINE SHALL BE LOCATED ONE FOOT FROM THE EDGE OF PAVEMENT ON EITHER SIDE; ON A ROADWAY MEASURING 26 FEET OR MORE IN WIDTH, TRAVEL LANES SHALL MEASURE 12 FEET AND THE WHITE EDGELINE SHALL BE LOCATED NO LESS THAN ONE FOOT FROM THE EDGE OF PAVEMENT ON EITHER SIDE. THIS SHALL BE STANDARD PRACTICE UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
6. PAPER JOINTS ARE TO BE PLACED BETWEEN DAYS OF PAVING OPERATIONS AS SPECIFIED IN THE STANDARD SPECIFICATIONS SECTION 610-11.
7. ALL MILLED AREAS WILL BE PAVED WITHIN 72 HOURS UNLESS APPROVED BY THE ENGINEER.
8. REPLACE ANY PORTION OF STOP BARS AND OTHER PAVEMENT MARKINGS AT ANY INTERSECTION INCLUDING Y-LINES NOT ACTUALLY BEING PAVED OVER, THAT ARE OBLITERATED BY THE PAVING OPERATION EITHER BY HAULING WHEEL TRACKS OR TACK TRUCK BY THE END OF EACH RESURFACING OPERATION



STEP 1



STEPS 2,3, & 4

- STEP 1 COVER EXISTING MANHOLE WITH APPROVED MATERIAL AND CONSTRUCT OVERLAY ACROSS TOP OF MANHOLE
- STEP 2 SAW CUT EXCAVATION AROUND MANHOLE 12" MIN. FROM MANHOLE FRAME.
- STEP 3 RAISE MANHOLE FRAME RINGS TO FINISH PAVEMENT PROFILE AND CROSS SLOPE.
- STEP 4 BACKFILL WITH EARLY STRENGTH P.C.C. TO DEPTHS AS DIRECTED.

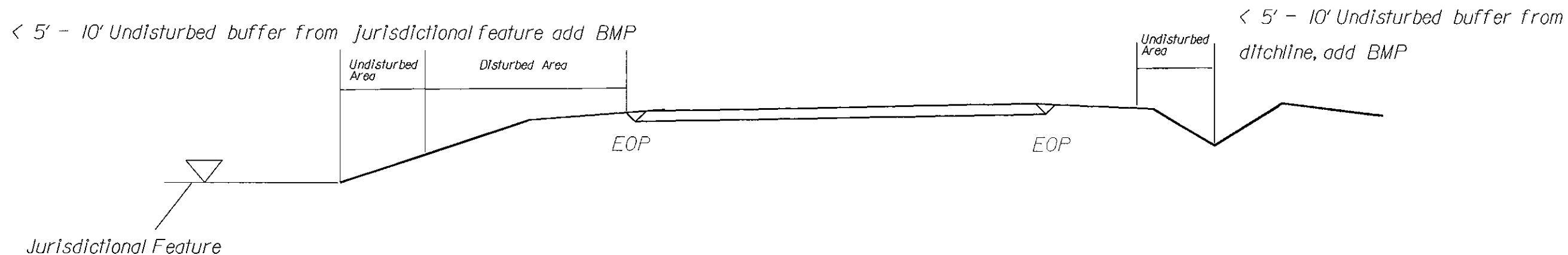
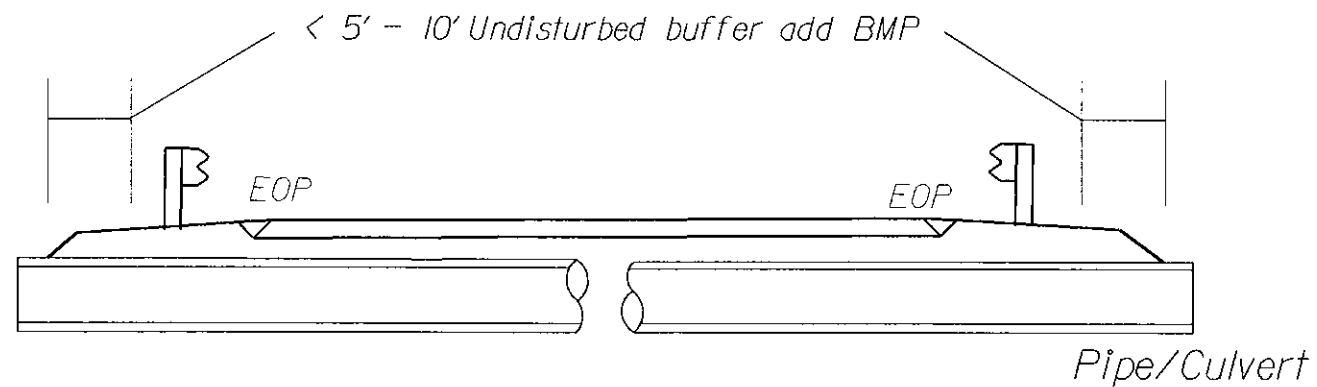
MANHOLE ADJUSTMENT DETAIL

NOTES: Less than 5' - 10' undisturbed buffer from ROW, ditchline, water feature, or drainage inlet, add BMP.

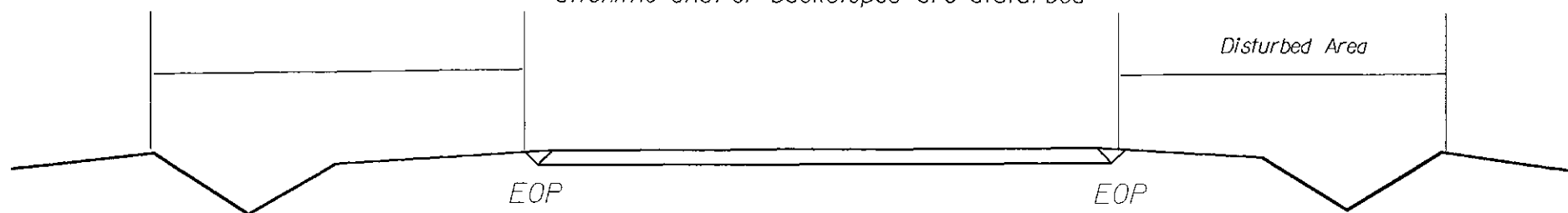
BMP Options: Wattle or Silt Fence

EROSION CONTROL DETAIL

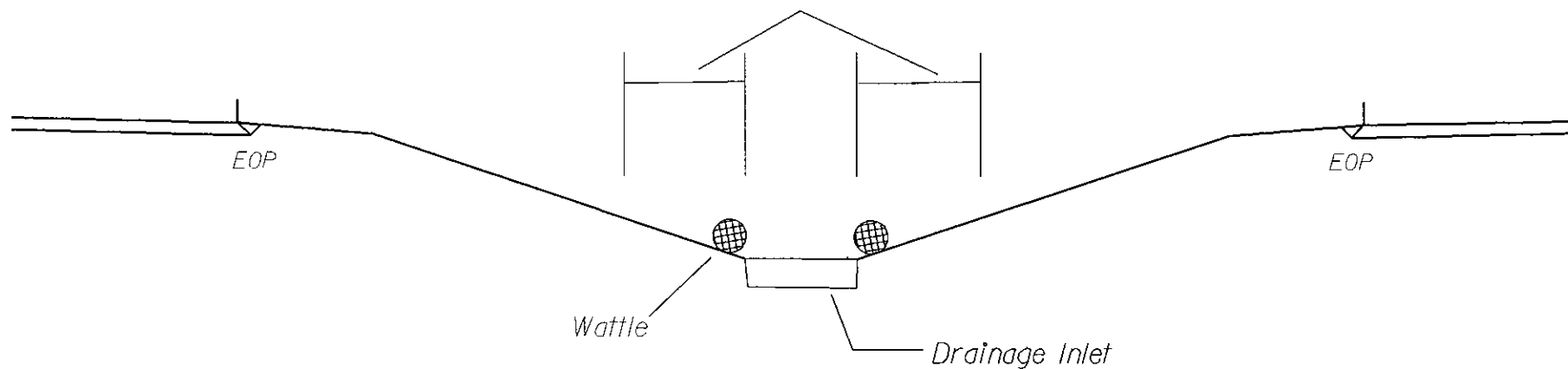
PROJECT REFERENCE NO.	SHEET NO.
2017CPT.09.28.10301.1 2017CPT.09.29.20301.1	12



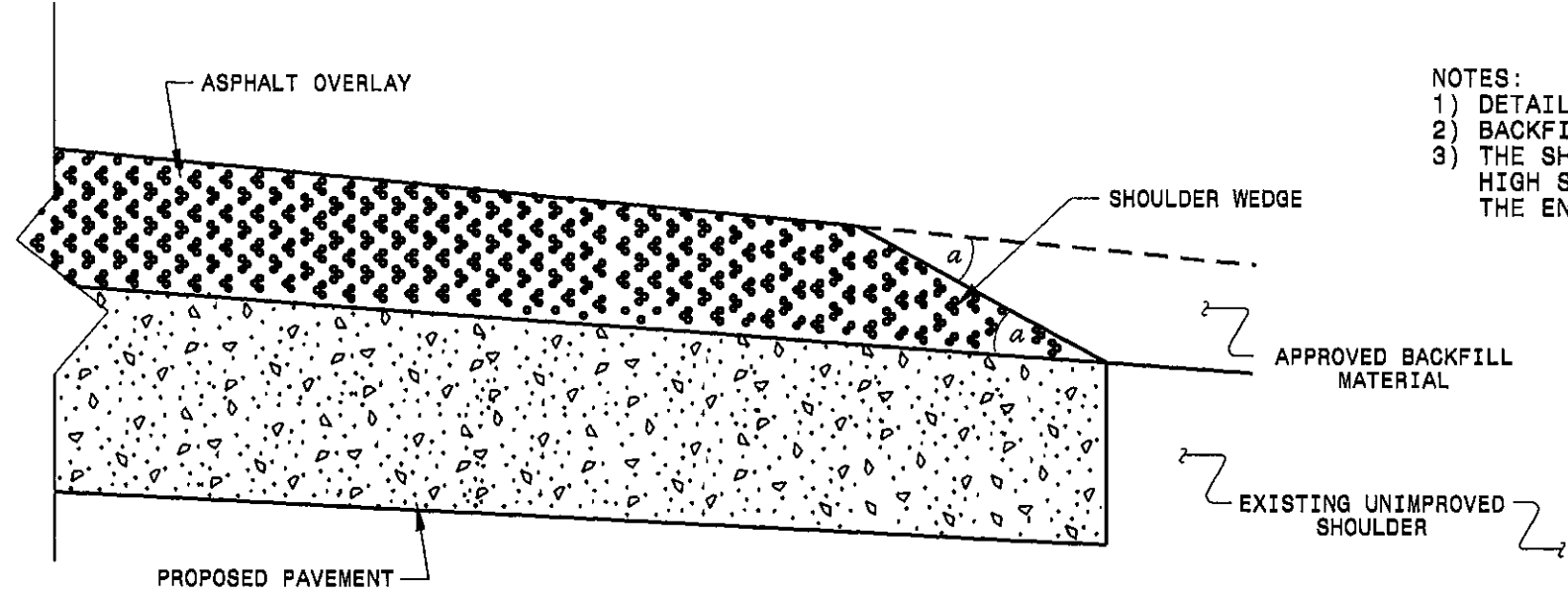
Use BMP's if shoulders and/or front slopes and/or ditchline and/or backslopes are disturbed



< 5' - 10' Undisturbed buffer from inlet, add wattle

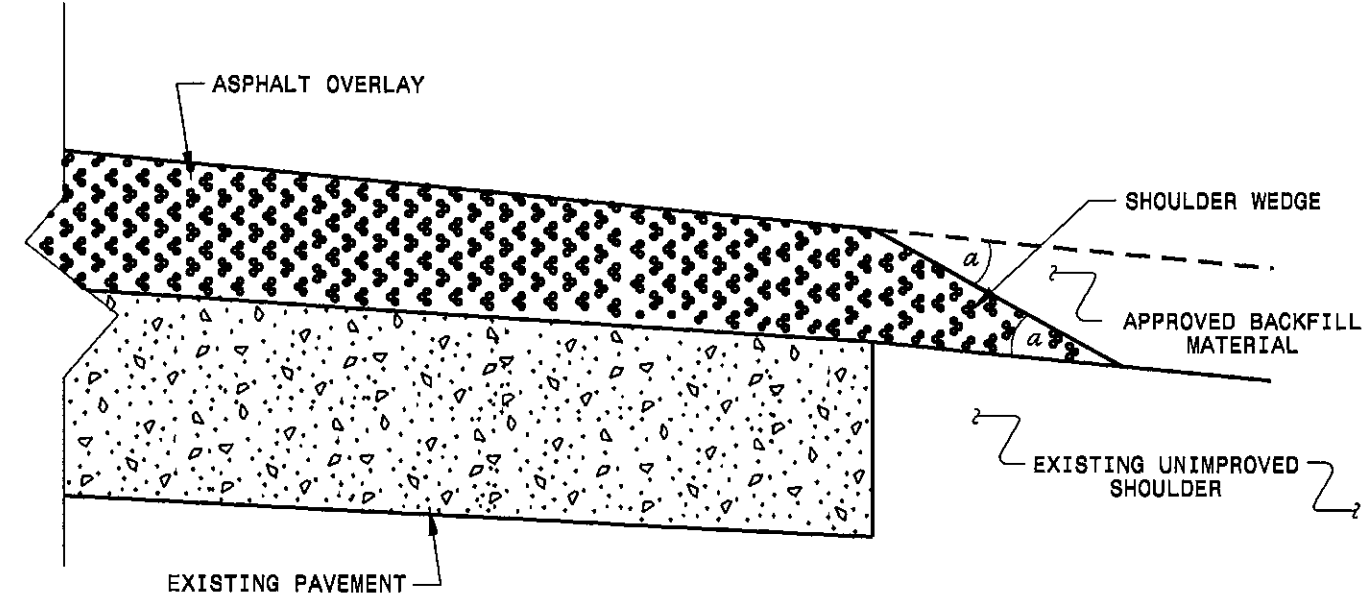


NOT TO SCALE

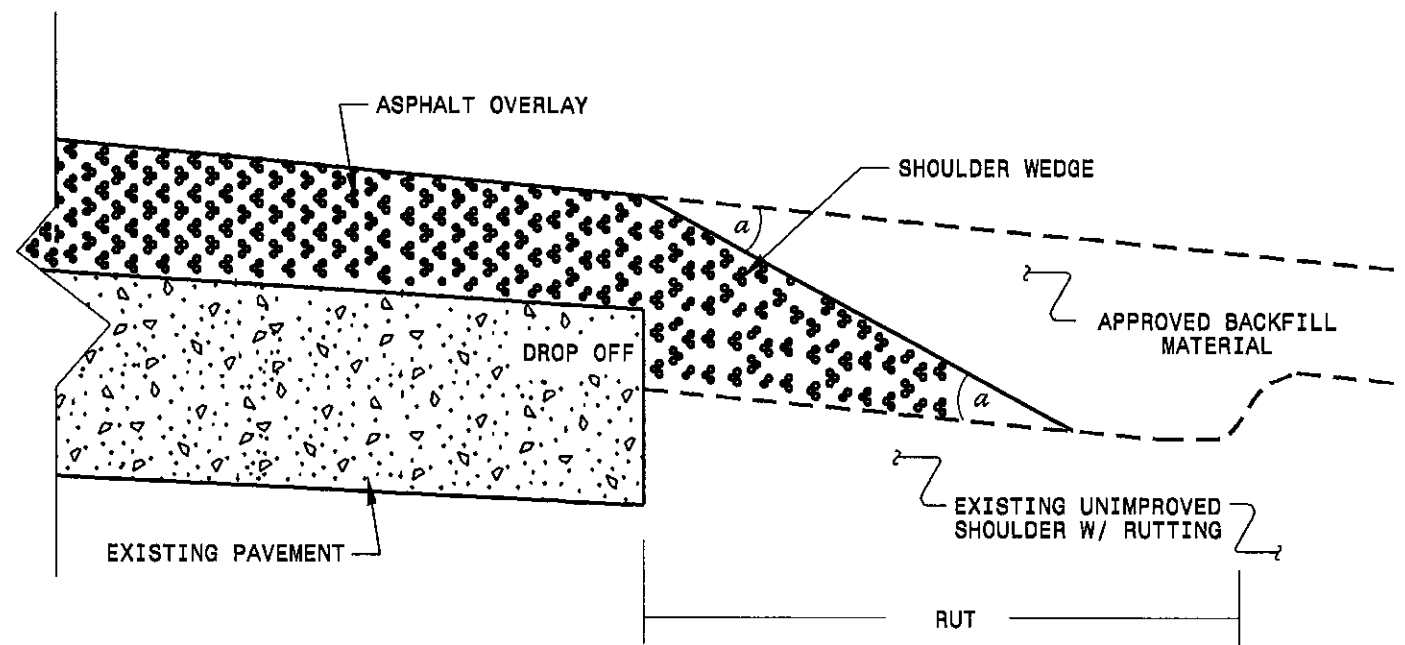


- NOTES:
- 1) DETAIL DOES NOT APPLY TO OGAFD AND ULTRA-THIN BONDED WEARING COURSE.
 - 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
 - 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS AND SIDE STREETS, HIGH SHOULDERS, AND OTHER LOCATIONS NOT FEASIBLE TO CONSTRUCT AS DIRECTED BY THE ENGINEER.

SHOULDER WEDGE DETAIL
(Resurfacing Projects w/ Widening or with Existing Paved Shoulder having no dropoffs)



SHOULDER WEDGE DETAIL
(Resurfacing Projects w/ NO Widening)



SHOULDER WEDGE DETAIL
(Resurfacing Adjacent to Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

CONTRACT STANDARDS AND DEVELOPMENT UNIT	
Office 919-707-6950	FAX 919-250-4119
SHOULDER WEDGE DETAILS	
ORIGINAL BY: T. SPELL	DATE: 7-19-11
MODIFIED BY:	DATE: 10/18/12
CHECKED BY:	DATE:
FILE SPEC: s:\uap\details\stand\shou\stand\std\std.dgn	

DAVIE Resurfacing Bridge List

									PROJECT NO.		SHEET NO.	
									2017CPT.09.28.10301 2017CPT.09.29.20301		14	
Map No.	Route No.	Route Name	Bridge No.	Feature Intersected	Floor Construction	Clear Roadway Width (Ft)	Horizontal Clearance Under (Ft.)	Vertical Clearance Under	2nd Opening Clearance Under	Length (Ft)	Posting	Recommended Treatment, From Bridge Maintenance
2	US 601	S. MAIN ST.	9	SOUTHERN RAILROAD	9 RC SLAB	31.8	NA	NA	NA	126	NA	DO NOT MILL DO NOT PAVE
3	US 64	LEXINGTON RD.	44	SOUTHERN RAILROAD	NA	NA	23.8	14 FT 05 IN	NA	70	NA	MAINTAIN CLEARANCE
4	SR 1410	FARMINGTON RD.	69	I-40	7 7/8 RC SLAB	34	NA	NA	NA	234	NA	DO NOT MILL DO NOT PAVE

PROJECT NO.	SHEET NO.	TOTAL NO.
2017CPT.09.28.10301,	15	
2017CPT.09.29.20301		

SUMMARY OF QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH MI	WIDTH FT	BORROW EXCAVATION CY	INCIDENTAL STONE BASE TONS	SHOULDER RECONSTRUCTION SMI	MILLING ASPHALT PAVEMENT, 2"DEPTH SY	MILLING ASPHALT PAVEMENT, 4"DEPTH SY	MILLING ASPHALT PAVEMENT, 0"TO 1 1/2" DEPTH SY	INCIDENTAL MILLING SY	INTERMEDIATE COURSE, I19.0B TONS	SURFACE COURSE, S9.5B TONS	SURFACE COURSE, SF9.5A TONS	SURFACE COURSE, S4.75A TONS	ASPHALT BINDER FOR PLANT MIX TONS	PATCHING EXISTING PAVEMENT TONS	AST MAT COAT # SY	ADJ. OF DROP INLET EA	ADJ. OF MANHOLES EA	ADJ. OF METER OR VALVE BOX EA	TEMPORARY SILT FENCE LF	WATTLE LF		
2017CPT.09.28.10301	Davie	1	NC 801	FROM PVMT JT AT RXR TRACK NEAR SR 1616 CORNATZER RD. TO EDGE OF PAVEMENT AT US 158	1,2	2	MU	NO	NO	4.414	VARIES 24-48	530	171	8.83						6,630				398	100				7	1,765	177	
TOTAL FOR MAP NO. 1										4.414		530	171	8.83						6,630				398	100				7	1,765	177	
2017CPT.09.28.10301	Davie	2	US 601	FROM US 64 E.O.P. TO APPROX. 1200 FEET NORTH OF BRIDGE #9 OVER SOUTHERN RXR	3,4	2	2WU	NO	NO	0.62	VARIES 22-35	65	60	1.09	8,562					1,002				60	20	8,562	1	5	3	218	22	
TOTAL FOR MAP NO. 2										0.62		65	60	1.09	8,562					1,002				60	20	8,562	1	5	3	218	22	
2017CPT.09.28.10301	Davie	3	US 64	FROM PAVEMENT JT WEST OF INTERSECTION OF US 601/US 158 TO APPROX. 100 FEET EAST OF RXR BRIDGE	5,6	2	2WU	NO	NO	0.338	VARIES 24-35	33	15	0.55	5,650					791				47	20			3	4	102	10	
TOTAL FOR MAP NO. 3										0.338		33	15	0.55	5,650					791				47	20			3	4	102	10	
2017CPT.09.28.10301	Davie	4	US 64	FROM APPROX. 100 FEET EAST OF RXR BRIDGE TO END OF TAPER EAST OF BETHEL CHURCH RD. SR 1602	1,2	2	2WU	NO	NO	1.033	VARIES 27-35	101	60	1.68	967					1,883				113	100			6	6	336	34	
TOTAL FOR MAP NO. 4										1.033		101	60	1.68	967					1,883				113	100			6	6	336	34	
TOTAL FOR PROJ NO. 2017CPT.09.28.10301										6.405		729	306	12.15	15,179			5,506	1,778		10,306				618	240	8,562	1	14	20	2,421	243
2017CPT.09.29.20301	Davie	5	FARMINGTON RD. SR 1410	FROM PUDDING RIDGE RD. SR 1435 TO US 158	1,2,6A	2	2WU	NO	NO	2.637	VARIES 22-33	299	255	4.99						3,988				296	20					997	100	
TOTAL FOR MAP NO. 5										2.637		299	255	4.99						3,988				296	20					997	100	
2017CPT.09.29.20301	Davie	6	SOUTH CLAYBON DR. SR 1467	FROM PAVEMENT JT. AT NC 801 TO END OF MAINTENANCE	7	2	2WU	NO	NO	0.432	18		51										265	18								
TOTAL FOR MAP NO. 6										0.432			51											265	18							
2017CPT.09.29.20301	Davie	7	JANLIN LANE SR 1476	FROM EDGE OF PAVEMENT AT S. CLAYBON DR. SR 1467 TO END	7	2	2WU	NO	NO	0.075	18		12										46	3								
TOTAL FOR MAP NO. 7										0.075			12											46	3							
2017CPT.09.29.20301	Davie	8	LESTER DR. SR 1477	FROM EDGE OF PAVEMENT AT S. CLAYBON DR. SR 1467 TO END	7	2	2WU	NO	NO	0.074	18		6										45	3								
TOTAL FOR MAP NO. 8										0.074			6											45	3							
2017CPT.09.29.20301	Davie	9	FREEDOM DR. SR 1478	FROM EDGE OF PAVEMENT AT S. CLAYBON DR. SR 1467 TO END	7	2	2WU	NO	NO	0.075	18		9										46	3								
TOTAL FOR MAP NO. 9										0.075			9											46	3							
2017CPT.09.29.20301	Davie	10	TURKEY FOOT RD. SR 1317	FROM SHEFFIELD RD. SR 1306 TO IREDELL COUNTY LINE	8	2	2WU	NO	NO	2.427	22	291	183	4.85				489					192	20						971	97	
TOTAL FOR MAP NO. 10										2.427		291	183	4.85				489						192	20					971	97	
2017CPT.09.29.20301	Davie	11	MARKLAND RD. SR 1618	FROM NC 801 TO FORK BIXBY RD. SR 1617	8	2	2WU	NO	NO	2.516	VARIES 20-21	302	192	5.03				467					221	20					1,006	101		
TOTAL FOR MAP NO. 11										2.516		302	192	5.03				467						221	20				1,006	101		
2017CPT.09.29.20301	Davie	12	COUNTRY LANE SR 1461	FROM EDGE OF PAVEMENT AT US 158 TO PAVEMENT JT. AT US 601	2,5A	2	2WU	NO	NO	2.232	21	268	174	4.46				547					163	20				1	893	89		
TOTAL FOR MAP NO. 12										2.232		268	174	4.46				547						163	20			1	893	89		
2017CPT.09.29.20301	Davie	13	CPP GLOBAL DR. / KAYDON DRIVE SR 1852	FROM SALISBURY ST. / US 601	9	2	2WU	NO	NO	0.22	VARIES 25-26	26		0.44				289					22	20					88	9		
TOTAL FOR MAP NO. 13										0.22		26		0.44				289						22	20				88	9		
2017CPT.09.29.20301	Davie	14	UNIFI DR. / AVGOL DR. SR 1853	FROM CPP GLOBAL DR. / KAYDON DRIVE SR 1852 TO TURN OUTS JUST WEST OF RXR CROSSING	9	2	2WU	NO	NO	0.258	VARIES 24-26	31		0.52				556					25	20				1	103	10		
TOTAL FOR MAP NO. 14										0.258		31		0.52				556						25	20			1	103	10		
TOTAL FOR PROJ NO. 2017CPT.09.29.20301										10.946		1,217	882	20.29			7,502	1,947	3,234	1,176	7,476	6,164	402	946	120			1	1	4,058	406	
GRAND TOTAL										17.351		1,946	1,188	32.44		15,179	7,502	7,453	5,012	1,176	17,782	6,164	402	1,564	360	8,562	1	15	21	6,479	649	

PROJECT NO.	SHEET NO.	TOTAL NO.
2017CPT.09.28.10301,	16	
2017CPT.09.29.20301		

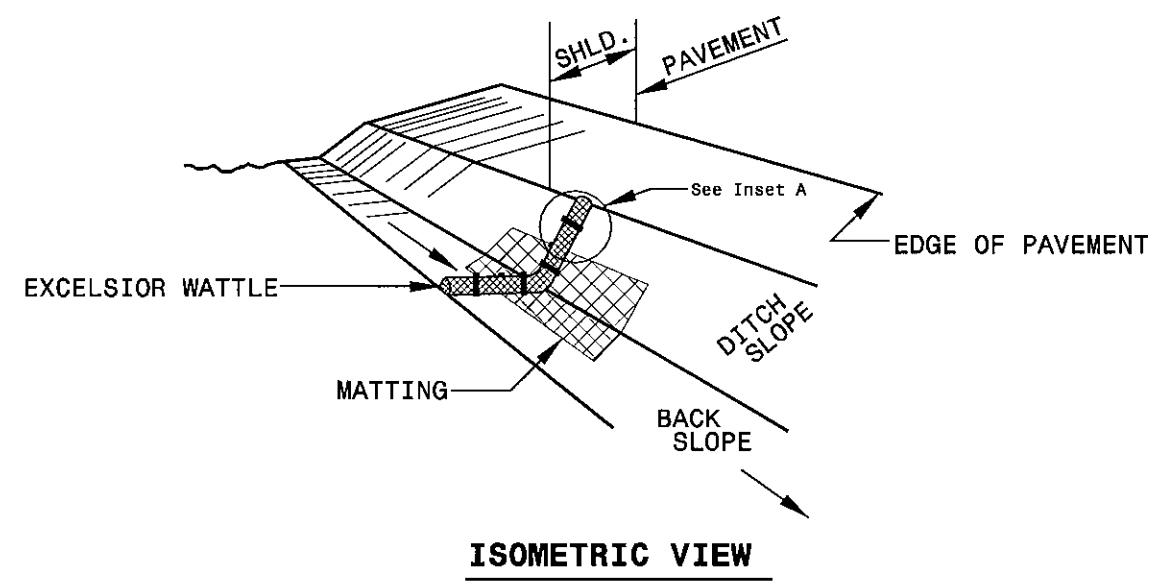
THERMOPLASTIC AND PAINT QUANTITIES

PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP	LANES	LANE TYPE	LENGTH	WIDTH	4413000000-E	4457000000-N	4685000000-E	4686000000-E	4695000000-E	4697000000-E	4702000000-E	4705000000-E	4710000000-E	4721000000-E	4725000000-E			4847000000-E		4905000000-N										
										WORK ZONE ADVANCE/GENERAL WARNING SIGNING	TEMPORARY TRAFFIC CONTROL	4" X 90 M WHITE THERMO	4" X 120 M YELLOW THERMO	4" X 120 M WHITE THERMO	8" X 90 M YELLOW THERMO	8" X 120 M WHITE THERMO	12" X 90 M YELLOW THERMO	THERMOPLASTIC PAVEMENT MARKING LINES (12", 120 MILS YELLOW) LF	16" X 120 M WHITE THERMO	24" X 120 M WHITE THERMO	THERMO RXR 120 M	THERMO MSG SCHOOL 120 M	THERMO LT ARROW 90 M	THERMO STR ARROW 90 M	THERMO RT ARROW 90 M	THERMO STR & RT ARROW 90 M	4" WHITE POLYUREA (HIGHLY REFLECTIVE ELEMENTS)	4" YELLOW POLYUREA (HIGHLY REFLECTIVE ELEMENTS)	SNOW PLOWABLE MARKERS						
2017CPT.09.28.10301	Davie	1	NC 801	FROM PVMT JT AT RXR TRACK NEAR SR 1616 CORNATZER RD. TO EDGE OF PAVEMENT AT US 158	1,2	2	MU	4.414	VARIES 24-48	1,575	1			427			50	55	2			10	3	8		49,338	53,191	291							
TOTAL FOR MAP NO. 1										1,575	1			427			50	55	2			10	3	8		49,338	53,191	291							
2017CPT.09.28.10301	Davie	2	US 601	FROM US 64 E.O.P. TO APPROX. 1200 FEET NORTH OF BRIDGE #9 OVER SOUTHERN RXR	3,4	2	2WU	0.62	VARIES 22-35									26					1		1	8,320	9,137								
TOTAL FOR MAP NO. 2																		26					1		1	8,320	9,137								
2017CPT.09.28.10301	Davie	3	US 64	FROM PAVEMENT JT WEST OF INTERSECTION OF US 601/US 158 TO APPROX. 100 FEET EAST OF RXR BRIDGE	5,6	2	2WU	0.338	VARIES 24-35										83					3		3	3,196	3,690	22						
TOTAL FOR MAP NO. 3																		83					3		3	3,196	3,690	22							
2017CPT.09.28.10301	Davie	4	US 64	FROM APPROX. 100 FEET EAST OF RXR BRIDGE TO END OF TAPER EAST OF BETHEL CHURCH RD. SR 1602	1,2	2	2WU	1.033	VARIES 27-35											214					8		1	9,190	13,974	68					
TOTAL FOR MAP NO. 4																			214					8		1	9,190	13,974	68						
TOTAL FOR PROJ NO. 2017CPT.09.28.10301										6,405	1,575	1		427				214			50	177	2		22	3	9	4		70,044	79,992	381			
																			214			2			38			150,036							
2017CPT.09.29.20301	Davie	5	FARMINGTON RD. SR 1410	FROM PUDDING RIDGE RD. SR 1435 TO US 158	1,2,6A	2	2WU	2.637	VARIES 22-33			28,374	32,556	909	584	214			1,598			124				7		2	468	468					
TOTAL FOR MAP NO. 5																28,374	32,556	909	584	214			1,598			124			7		2	468	468		
2017CPT.09.29.20301	Davie	6	SOUTH CLAYBON DR. SR 1467	FROM PAVEMENT JT. AT NC 801 TO END OF MAINTENANCE	7	2	2WU	0.432	18																										
TOTAL FOR MAP NO. 6																																			
2017CPT.09.29.20301	Davie	7	JANLIN LANE SR 1476	FROM EDGE OF PAVEMENT AT S. CLAYBON DR. SR 1467 TO END	7	2	2WU	0.075	18																										
TOTAL FOR MAP NO. 7																																			
2017CPT.09.29.20301	Davie	8	LESTER DR. SR 1477	FROM EDGE OF PAVEMENT AT S. CLAYBON DR. SR 1467 TO END	7	2	2WU	0.074	18																										
TOTAL FOR MAP NO. 8																																			
2017CPT.09.29.20301	Davie	9	FREEDOM DR. SR 1478	FROM EDGE OF PAVEMENT AT S. CLAYBON DR. SR 1467 TO END	7	2	2WU	0.075	18																										
TOTAL FOR MAP NO. 9																																			
2017CPT.09.29.20301	Davie	10	TURKEY FOOT RD. SR 1317	FROM SHEFFIELD RD. SR 1306 TO IREDELL COUNTY LINE	8	2	2WU	2.427	22			26,115	25,629	56																					
TOTAL FOR MAP NO. 10																26,115	25,629	56																	
2017CPT.09.29.20301	Davie	11	MARKLAND RD. SR 1618	FROM NC 801 TO FORK BIXBY RD. SR 1617	8	2	2WU	2.516	VARIES 20-21			27,072	26,569	16																					
TOTAL FOR MAP NO. 11																27,072	26,569	16																	
2017CPT.09.29.20301	Davie	12	COUNTRY LANE SR 1461	FROM EDGE OF PAVEMENT AT US 158 TO PAVEMENT JT. AT US 601	2,5A	2	2WU	2.232	21			24,016	24,210	165			47									5									
TOTAL FOR MAP NO. 12																24,016	24,210	165			47														
2017CPT.09.29.20301	Davie	13	CPP GLOBAL DR. / KAYDON DRIVE SR 1852	FROM SALISBURY ST. / US 601	9	2	2WU	0.22	VARIES 25-26																										
TOTAL FOR MAP NO. 13																																			
2017CPT.09.29.20301	Davie	14	UNIFI DR. / AVGOL DR. SR 1853	FROM CPP GLOBAL DR. / KAYDON DRIVE SR 1852 TO TURN OUTS JUST WEST OF RXR CROSSING	9	2	2WU	0.258	VARIES 24-26				2,724																						
TOTAL FOR MAP NO. 14																	2,724																		
TOTAL FOR PROJ NO. 2017CPT.09.29.20301										10,946						105,577	111,688	1,146	584	214	47	1,598	100	73	4		12		12		2	2		468	468
GRAND TOTAL										17,351	1,575	1				112,834	112,834	1,146	1,011	214	261	1,598	150	442	6	12	34	3	11	6	70,512	80,460	381		

NOTE: All Quantities listed include turn lanes and are estimates; Payment will be based on actual field measurements and quantities received.

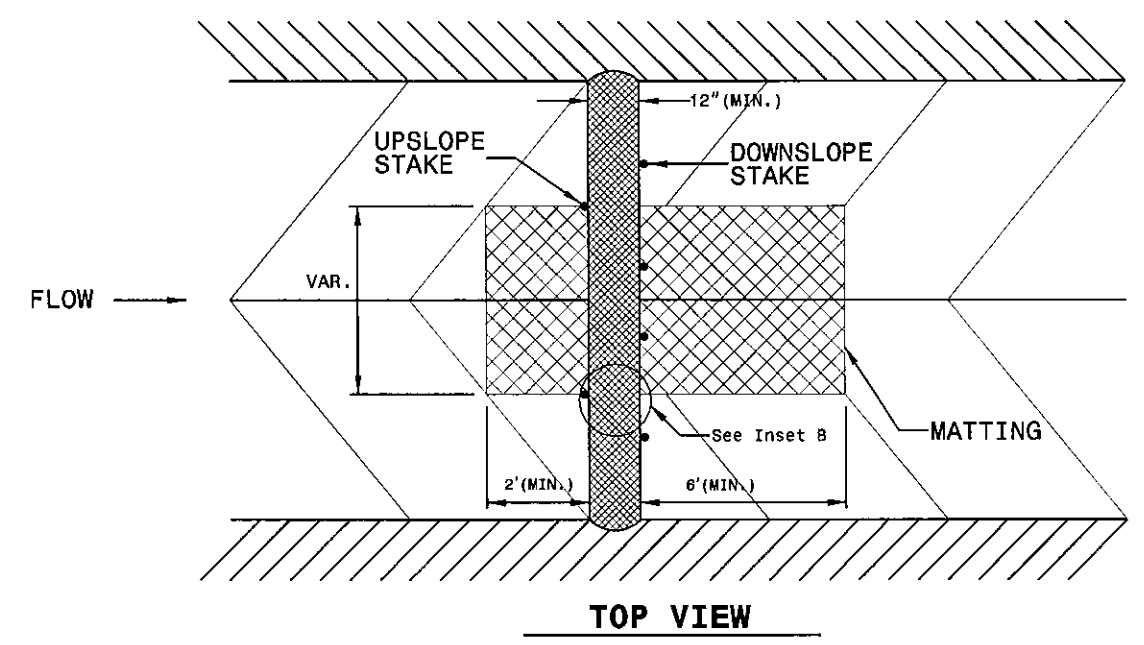
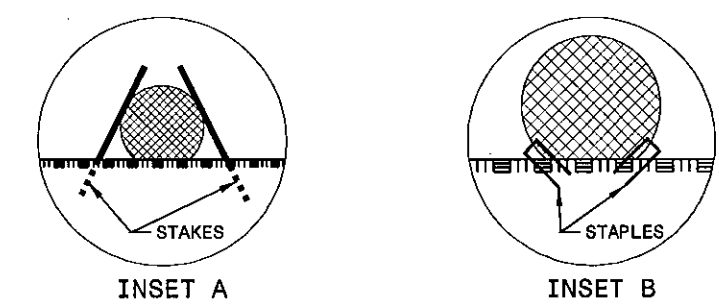
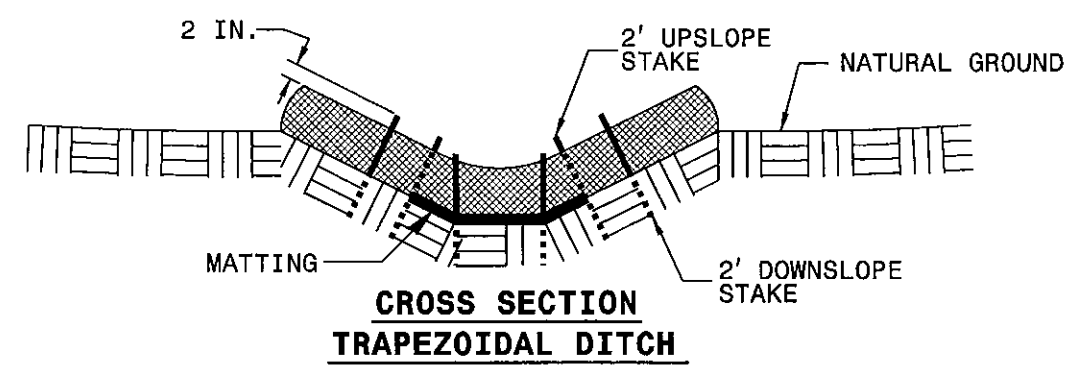
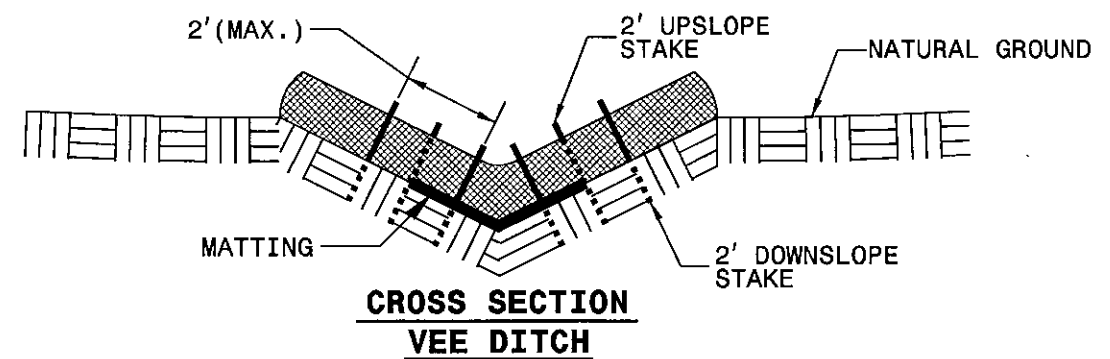
PROJECT REFERENCE NO. X-XXXX	SHEET NO. EC-2G
RW SHEET NO.	
ROADWAY DESIGN ENGINEER	HYDRAULICS ENGINEER

WATTLE DETAIL

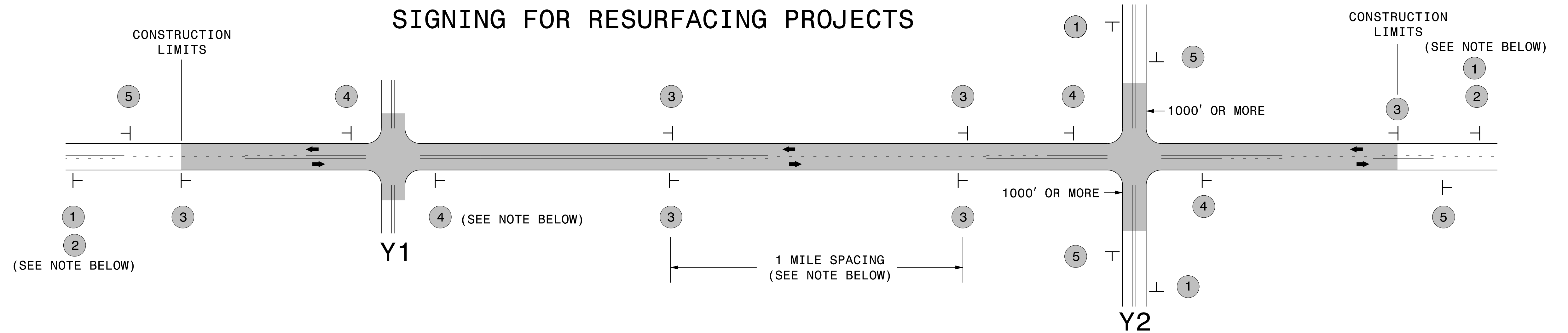


NOTES:

- USE MINIMUM 12 IN. DIAMETER EXCELSIOR WATTLE.
- USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. NOMINAL CROSS SECTION.
- ONLY INSTALL WATTLE(S) TO A HEIGHT IN DITCH SO FLOW WILL NOT WASH AROUND WATTLE AND SCOUR DITCH SLOPES AND AS DIRECTED.
- INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.
- PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12" IN LENGTH.
- INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.
- INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.



SIGNING FOR RESURFACING PROJECTS



LEGEND	
┆	STATIONARY SIGN
←	DIRECTION OF TRAFFIC FLOW

MAINLINE (-L-) SIGNING

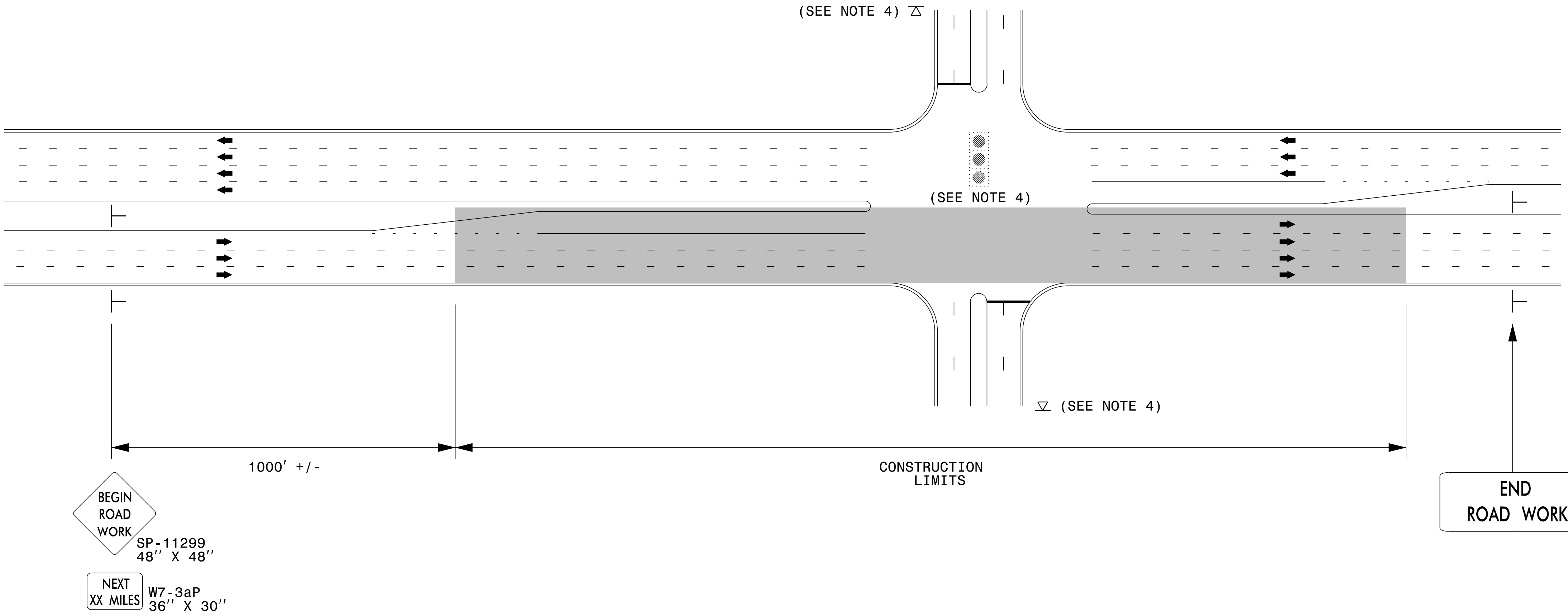
-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	MAINLINE (-L-) SIGNING		-Y- LINE SIGNING	
	1	 W20-1 48" X 48"	PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> LESS THAN 1000' OF RESURFACING ALONG -Y- LINE SUBDIVISION ROADS DEAD END ROADS <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div> W20-1 48" X 48" </div> <div> W20-7 A 48" X 48" </div> </div> <p>PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</p>
	2	 W7-3aP 24" X 18"	#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)	
	3	 SP 13107 48" X 48"	PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. IF NO -Y- LINES EXIST, PLACE 2ND SET 1/2 MILE FROM THE CONSTRUCTION LIMITS AND THEN SPACE 1 MILE THEREAFTER.	
	4	 SP 13106 48" X 48"	THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.	
5	 G20-2 A 48" X 24"	PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS.		

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**RESURFACING
ADVANCE WARNING SIGNS
FOR
RURAL AND SUBURBAN
2 LANE ROADWAYS**

URBAN / SUBURBAN WORKZONES



NOTES:

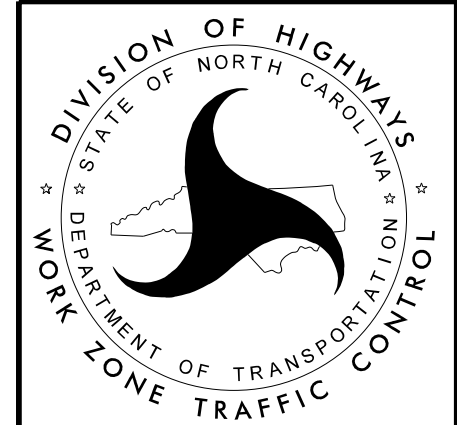
- 1) 48" x 48" SIZED SIGNS (SP- 11299) MAY BE REDUCED TO 36" X 36" ON ROADWAYS WITH SPEED LIMITS OF 40 MPH OR LESS.
- 2) MOUNT SIGNS THAT ARE LARGER THAN 10 SQUARE FEET IN AREA ON TWO OR MORE WOOD OR U-CHANNEL SUPPORTS. PERFORATED SQUARE TUBING SUPPORT SYSTEMS MAY SUPPORT LARGER AREAS ON A SINGLE SUPPORT. FOLLOW MANUFACTURER'S RECOMMENDATIONS. THESE SYSTEMS SHALL BE NCHRP 350 COMPLIANT AND NCDOT APPROVED.
- 3) ADVANCE WARNING SIGNS NOT REQUIRED ON NON-SIGNALIZED SIDE STREETS.
- 4) MAY USE LAW ENFORCEMENT TO CONTROL TRAFFIC AT SIGNALIZED INTERSECTIONS AS DIRECTED BY THE ENGINEER. PROVIDE PORTABLE "ROAD WORK AHEAD" (W20-1) SIGNS 500' IN ADVANCE ALONG BOTH APPROACHES FROM THE SIDE STREETS WHEN PAVING PROCEEDS THROUGH THE INTERSECTION.
- 5) LATERAL CLEARANCE AT ALL SIGN LOCATIONS SHALL BE 2' AS MEASURED FROM THE EDGE OF PAVEMENT OR THE FACE OF THE CURB. WHEN UNABLE TO OBTAIN THE LATERAL CLEARANCE WITHIN THE MEDIAN AREA USE SHOULDER MOUNTS ONLY.
- 6) SIGN MOUNT LOCATIONS SHALL NOT BLOCK SIDEWALKS OR DRIVEWAYS.
- 7) IF STATIONARY GENERAL WARNING SIGNS ARE USED, THEY WILL BE PAID FOR PER SECTION 104 OF THE NCDOT STANDARD SPECIFICATIONS AS EXTRA WORK.
- 8) IF MILLED AREAS ARE NOT PAVED BACK BY THE END OF THE WORK DAY, PORTABLE SIGNS SHALL BE USED TO WARN DRIVERS OF THE PRESENT CONDITIONS. THESE ARE TO INCLUDE, BUT NOT LIMITED TO "ROUGH ROAD" W8-8, "UNEVEN LANES" W8-11, "GROOVED PAVEMENT" W8-15 w/MOTORCYCLE PLAQUE MOUNTED BELOW. THESE ARE TO BE DOUBLE INDICATED ON MULTI-LANE ROADWAYS WITH SPEED LIMITS 45 MPH AND GREATER WHERE LATERAL CLEARANCE CAN BE OBTAINED WITHIN THE MEDIAN AREAS. THESE PORTABLE SIGNS ARE INCIDENTAL TO THE OTHER ITEMS OF WORK INCLUDED IN THE TEMPORARY TRAFFIC CONTROL (LUMP SUM) PAY ITEM.

LEGEND

┆ STATIONARY SIGN

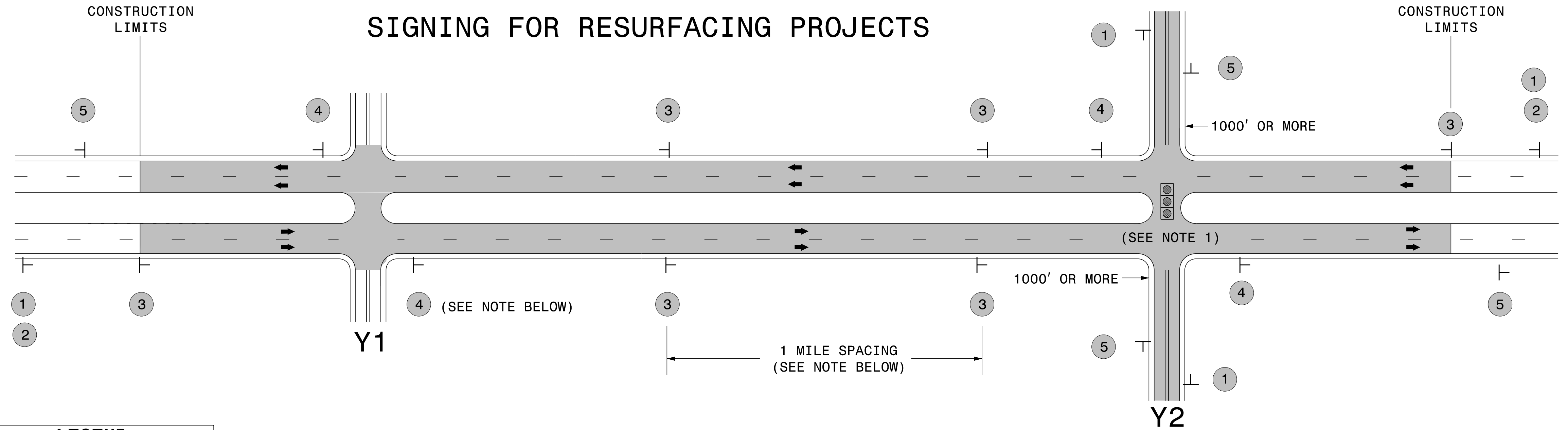
➔ DIRECTION OF TRAFFIC FLOW

END ROAD WORK
G20-2 A
48" X 24"



RESURFACING ADVANCE WARNING SIGNS FOR URBAN / SUBURBAN FACILITIES

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LEGEND
 ┆ STATIONARY SIGN
 ← DIRECTION OF TRAFFIC FLOW

MAINLINE (-L-) SIGNING

-Y- LINE SIGNING

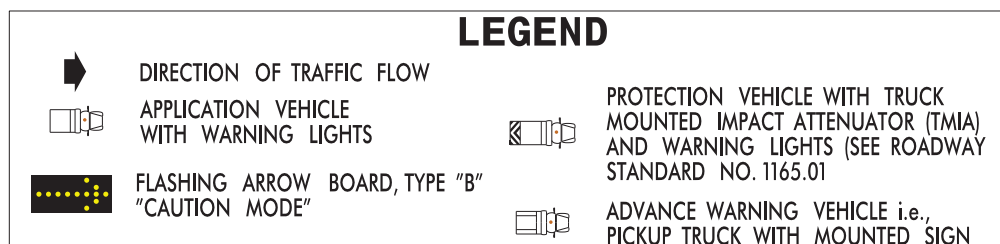
SIGNING NOTES AND PLACEMENT PER DIRECTION	 	<p>PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.</p> <p>#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)</p>	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div style="text-align: center;"> <small>W20-1 48" X 48"</small> </div> <div style="text-align: center;"> <small>W20-7 A 48" X 48"</small> </div> </div> <p>PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</p> <p>NOTES:</p> <ol style="list-style-type: none"> 1) MAY USE LAW ENFORCEMENT TO CONTROL TRAFFIC AT SIGNALIZED INTERSECTIONS AS DIRECTED BY THE ENGINEER. PROVIDE PORTABLE "ROAD WORK AHEAD" (W20-1) SIGNS 500' IN ADVANCE ALONG BOTH APPROACHES FROM THE SIDE STREETS WHEN PAVING PROCEEDS THROUGH THE INTERSECTION.
		<p>PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. IF NO -Y- LINES EXIST, PLACE 2ND SET 1/2 MILE FROM THE CONSTRUCTION LIMITS AND THEN SPACE 1 MILE THEREAFTER.</p>	
		<p>THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.</p>	
		<p>PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS.</p>	

3/23/2015
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**RESURFACING
 ADVANCE WARNING SIGNS
 FOR RURAL AND SUBURBAN
 MULTI-LANE ROADWAYS
 W/ SHOULDER SECTIONS**

Notes on Moving Operation Caravan for Placing Pavement Marking or Markers on Four Lanes or More of a Multi-Lane Roadway

- (1) The following options may be used as the first advance warning the motorists see:
 - a. Truck mounted advance warning signs
 - b. Truck mounted changeable message sign (CMS)
 - c. Ground mounted advance warning signs
(Must circle to pick up signs)
 - d. Ground mounted changeable message sign (CMS)
(Must circle to pick up signs)
- (2) All advance warning signs must be 48" x 48" with fluorescent orange type VII, VIII, or IX sheeting. If space limitations on shoulder prohibit a 48" x 48" sign, a smaller sign can be used with approval from engineer.
- (3) Signs on vehicles should be mounted a minimum of one foot from the ground and should not block the motorist's sight of the flashing arrow board and/or warning lights.
- (4) Ground mounted advanced warning signs should be mounted a minimum of five feet from the ground to the bottom of the sign.
- (5) Sign spacing should be adjusted for horizontal and vertical curves, etc. to improve sight distances.
- (6) Additional vehicles should be used in work caravan to facilitate drying of pavement marking material (TMA's are optional on these additional vehicles). However, the first vehicle motorists see in the travel lane shall have a TMA.
- (7) Adjust distances as needed to prevent motorists from entering space between the application and protection vehicle. Distance can be lengthened to accommodate sight distance needs.
- (8) Round up mileage to next whole mile. Work zone should not exceed five miles in length.
- (9) Radio communication between vehicles is required.
- (10) Use of warning lights on all vehicles if preferred, but a rotating beacon may be used instead.
- (11) If work is performed at night, the work area must be illuminated with machine and/or tower lights as approved by engineer.
- (12) All traffic control devices will be considered incidental to the pay items for pavement marking and markers.
- (13) Informational signs should be activity specific, i.e. "Paint Crew in Road". Signs may be rectangular or diamond shape. Sign size should be based on the motorist ability to recognize sign when traveling five miles above posted speed limit.



Moving Operation Caravan

(Operations Traveling 3 mph or Faster)
 Placing Pavement Marking or Markers
 On Four Lanes or More of a Multi-Lane Roadway

